



# THE PROMENADE SPECIAL AREA PLAN

PREPARED FOR SEOPW CRA AND THE CITY OF MIAMI DEPARTMENT OF PLANNING AND ZONING JANUARY 2004



With its central location, the Promenade has the potential to become a regional attraction.

## **Table of Contents**

---

1.0 Introduction

2.0 Historic Connections and Current Dynamics

3.0 The Concept

4.0 The Five-Year Transformation

5.0 The Plan

6.0 Implementation

7.0 Design Guidelines

- 7.1 Increasing Development Potential and Variations
- 7.2 Guidelines for Redevelopment
  - Building Dispositions
  - Building Configuration
  - Building Type Standards / Land Uses
  - Parking Standards
  - Additional Design Standards
- 7.3 Guidelines for Renovations
  - Simple Improvements
- 7.4 Signage

8.0 Glossary of Terms

## 1.0 Introduction

The Promenade will add a new distinctive, informal urban character to downtown Miami while reinforcing a sense of connection to the city's historic economic growth. Thanks to the recent creation of the Park West Entertainment District, these blocks are now a major location for redevelopment. The Promenade Special Area Plan outlines unambiguous design details and implementation measures to channel the private and public redevelopment efforts to create a new kind of bustling commerce and social activity.

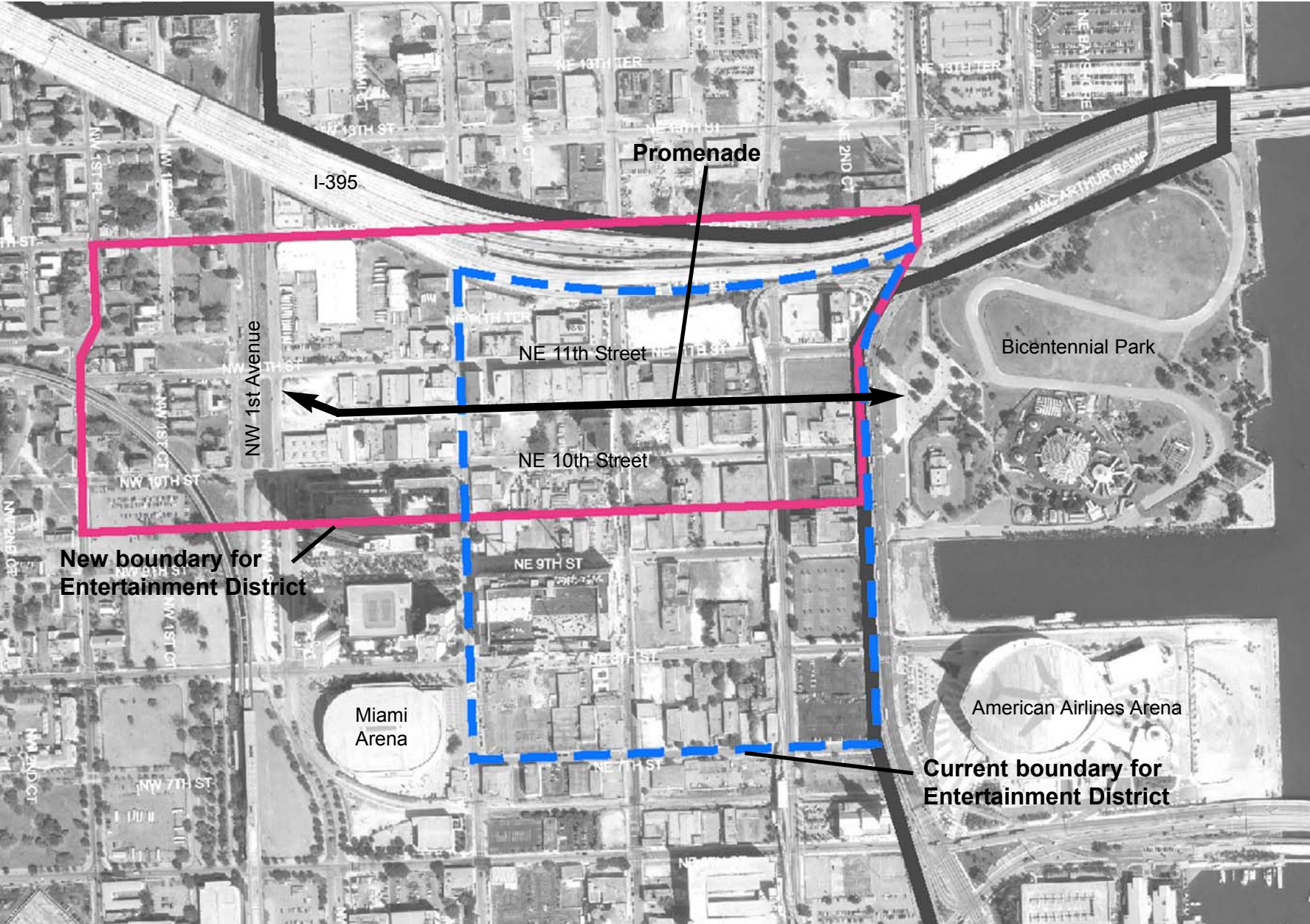
The Promenade is located within the recently designated Park West Entertainment District, and is envisioned as its vibrant centerpiece. Redevelopment of the Promenade should cultivate a genuine 24-hour environment where people can be entertained, create, work, and reside— a place that celebrates Miami's diverse popular culture and music scene.

Redevelopment of the Promenade should cultivate a genuine 24-hour environment where people can be entertained, create, work and reside.



The Promenade is currently characterized by blank walls, large expanses of asphalt, and overhead cobra light fixtures that are usually found on high speed roadways.





## 2.0 Historic Connections and Current Dynamics

The Promenade occupies the former railroad right-of-way located mid-block between 10th and 11th streets from Biscayne Boulevard to Northwest 1st Avenue. This railway once linked the working waterfront to the original Port of Miami, located at the time, on land now occupied by Bicentennial Park. Goods were loaded and unloaded in the adjacent warehouse buildings and transported on rail to and from the Port. The Promenade's unique locale offers an opportunity to connect to the history of the working waterfront through creative adaptive re-use of the right-of-way and old warehouse buildings.

Today, many of the warehouse buildings sit empty or abandoned. When the Port was relocated and the railway abandoned, the adjacent area of warehouses and commercial services declined. For many years, the area was neglected. Efforts to stimulate re-use and redevelopment of the area have occurred in recent decades. The Special Area Plan is the latest collaborative effort to enact an effective plan and comprehensive strategy for the redevelopment of the Promenade.

The Entertainment District is bounded on the north by the south side of I-395, on the south by Seventh Street, and from Biscayne Boulevard on the east to Miami Avenue on the west.

The designation of the Entertainment District by the City of Miami has helped to stimulate redevelopment efforts. This policy allowed for the allotment of eleven liquor licenses within the area, with permission for hours of operation that are longer than those in competing areas. As a result, a first generation of pioneers has invested in the area, a dynamic group of entrepreneurs which are turning existing warehouse buildings into nightclubs. Though the Entertainment District covers a broad area, these businesses have begun to develop predominantly in one location, along Eleventh Street. The clubs have already begun to create a lively after-hours scene. In addition, redevelopment energy is moving west of the Entertainment District as several eateries are setting up businesses. These new businesses, which are not necessarily reliant on 24-hour liquor licenses, can be part of the after-hours scene as well as capture the business of nearby residents during the daytime and evenings.

The Promenade area is currently functioning as an alley running east-west behind the warehouses. Garbage pickup is handled at the rear of the buildings. During the day the space is used for some parking.

At night, on the other hand, when the clubs are active, the Promenade is used by

club attendees. At that time Eleventh Street becomes a drop-off location for taxis and motorists. Its narrow sidewalks become filled with people lining up to enter the clubs; the Promenade behind the clubs offers the only reprieve of additional space to accommodate the club-goers who want to step outside for fresh air or to socialize.

The nightlife in Park West is an example of the growing dynamic 24-hour environment. Many people occupy this area throughout the night and morning as shown in this photo taken at 7:00 a.m.



### 3.0 The Concept

The goal for the economic and cultural redevelopment of the Promenade is to create a 24-hour environment built around the after-hours entertainment venue that is currently developing. With the right mix of uses and activities, the Promenade can be a "celebrated place" of street culture, ethnicity, and artistic diversity. This vibrant street life is what makes city living attractive, exciting, and decidedly different from a passive suburban lifestyle. The intention of Promenade redevelopment is to create a place where people can live, work, and play.

Through the public process for the planning of Bicentennial Park, community members expressed the desire to establish a meaningful way to link nearby neighborhoods to the Park. The Promenade will eventually consist of a series of interconnected pedestrian spaces which lead from Bicentennial Park to Overtown. In the tradition of pedestrian streets found in European and pre-World War II cities, the Promenade is envisioned as a series of courtyards and plazas connected by pedestrian "paseos" or passages. Florida examples include the courtyards of Via Mizner adjacent to Worth Avenue in Palm Beach (right), and the passages that connect Center Street to Park Avenue in Winter Park.

Each of the Promenade's public spaces will have its own feel and scale. These spaces will provide a place to take time out from the noisy clubs, dine outside, or queue for special performances. They also offer opportunities for public art and interpretive installations that are relevant to Miami's heterogeneous culture.

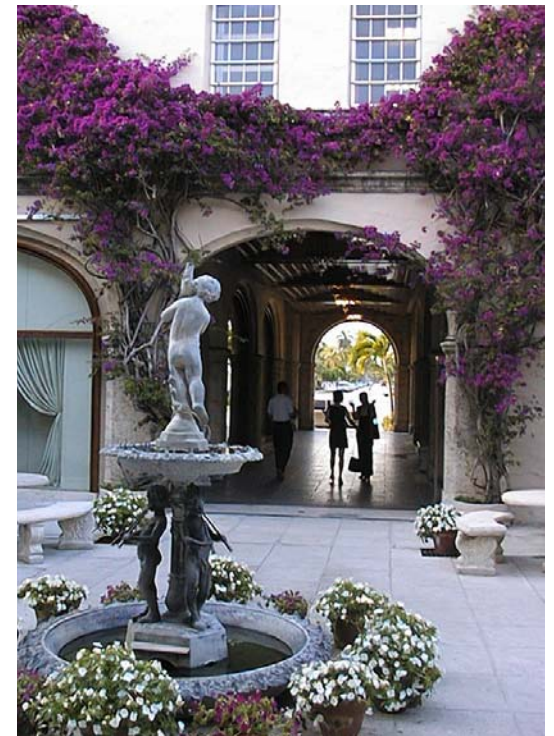
Redevelopment is proposed in a compact form which will allow new buildings to blend with existing structures. In addition, both new and old buildings should accommodate a mix of uses.

As day turns to night, mixed uses should make it possible to achieve a "chameleon" effect: the economics of the Promenade will shift from one set of customers to another. This will be done by mixing the first-floor entertainment venues with more standard retailing of a convenience-goods nature that will be supported by the apartment dwellers, some office uses, and others during the daytime hours. The ground floor levels will focus on entertainment and retailing; the second level also offers opportunities for bars and clubs with galleries, balconies, and verandas naturally suited for people watching. This apartment market is typically more attractive to a highly skilled and creative class

of artists, entertainers, shopkeepers, and young professionals<sup>1</sup>.

1 - "Economic Feasibility for Revitalization of the Park West Entertainment District Community Redevelopment Area" by ZHA, Inc. Annapolis, MD.

Paseos and courtyards provide a quiet escape from the noise of the city.





Alley between 10th and 11th Streets: Existing Conditions.



The first round of improvements is to include uniform paving material, lighting, and initial façade treatments.





Lanterns and signs brighten the scene. Storefronts replace blank walls.



The Promenade, over time: New Buildings will be added, and additional properties transformed.

## 4.0 The Five Year Transformation

Longevity of the Entertainment District will require two critical elements:

- #1 - Well distributed after-hours venues.
- #2 - Establishing a 24-hour mix of uses.

Currently, the banding of after-hours venues along one block is creating an increasingly intense concentration of traffic and parking demands in one location at certain times. This situation is being tolerated by the initial group of venues with the expectation of improved infrastructure. However, as more venues come online, the event traffic and parking experience may be too intense and not tolerated by the clientele. The concentration of after-hours venues in the first area may eventually hurt the emerging district. Correspondingly, if the district is to develop into a 24-hour environment, blocks with after-hours-only venues situated side by side result in a dead street scene during the daytime and early evening. Entertainment districts adapt and are interesting when there is life and activity on the street throughout most of the day. It is therefore vital to promote a variety uses and destinations distributed along the street.

### Five Year Goal

Within the next five years the goal is to have a range of new projects implemented.

The intent is to demonstrate the "immersive environment" of improved streets with buildings on both sides which form a complete 360-degree experience. This scene will be built up from a series of projects that are varied in use, so therefore the focus should be placed on adding new apartments, places of work, and neighborhood services. These will bring more people, activity, and street life to the district.

The computer visualization (previous page) depicts physical improvements both within the Promenade and on the adjacent private properties.

### 4.1 Walkways

The walkways of the Promenade should be designed specifically for pedestrian use and to accommodate occasional vehicular access for service needs. Pedestrian-oriented narrow gauge rail for cars about six-feet wide could also be accommodated within certain key routes, linking parking structures and neighboring areas to the Promenade. Like other pedestrian streets, the grade of the surface is to be maintained at sidewalk level. Engineering work will be required to confirm drainage details, but it is anticipated that drainage will be accommodated by a set of single French drains, located in the center of

the right-of-way, arrayed in a linear pattern along the Promenade.

### 4.2 Outdoor Dining

Outdoor dining should be encouraged to encroach upon the space of the Promenade, adjacent to the buildings. A clear area of minimum 12' width for walking should be reserved in the center. Parking shuttle vehicles will require additional width. The concrete surface should be differentiated through color and / or scoring patterns to delineate the two areas.

### 4.2 Inviting Façades & Entrances

The Promenade right-of-way is currently faced by the backs of building. The rear façades of these warehouse buildings are generally simple and were utilitarian responses to the buildings' original industrial purpose, but they are inadequate for the Promenade public space. They have little or no glazing and almost no projecting elements such as awnings. These blank façades create a "back of house" appearance which is un-welcoming. As buildings are improved and new buildings are constructed, they must present a proper façade onto the Promenade.

The design details which will create an inviting façade include:

## Transparency

The façades at ground level provide a large amount of transparency or glazing. They should be designed as shopfront or restaurant windows, which typically have seventy percent (70 %) or more transparency.

## Projecting elements

Awnings, marquees, colonnades, cantilevered balconies, roof overhangs, and galleries help create identity at street level for each building, as well as provide shade and protection from weather.

## 4.5 Sense of Entry

The entrance to the Promenade at each intersection is currently defined by the sides of buildings (This is generally the case, with the noteworthy exception of Miami Avenue). A variety of design elements will assist to create a sense of entry at each block:

**Upgraded Façades-** New doors and windows can create a proper front façade through architectural detailing.

**Light Fixtures-** A pair of light fixtures is to be used to frame each entry point. These light fixtures will intentionally be larger in scale and more substantial than the fixtures used elsewhere along the Promenade.

**Advertising and Signage-** Electrified advertising and multimedia elements, in the form of large format, rear-lit images, video displays or "plasma" screens, will create the needed visual impact. They will also convey to the outdoors the sense of action and excitement on the interior of the building, and relieve the negative impact of blank walls. Signs for the corner businesses should face onto the north-south avenues. In this concentrated area, an amalgam of bright, exuberant signage is central to the design idea. Adding sign elements to otherwise undistinguished buildings will also be a quick, affordable way to transform the grim scene to an upbeat, unique one. Therefore, visually prominent signs should be encouraged; if necessary, the City should execute waivers, exceptions, or exemptions from standard citywide sign regulations along the Promenade.

Intersections on Lincoln Road are narrowed to slow through traffic and provide safety for the pedestrian.



## 4.6 Intersection Design

Careful design of the intersections of the Promenade with the north-south avenues is essential for pedestrians to feel comfortable crossing the Avenues. At the same time, motorists need to be made aware they are passing through an area where there are many pedestrians. Motorists need visual cues, prior to arriving at the intersection, that this is an area where they must slow down. This can be accomplished by visually "pinching" the road space, narrowing drivers' sight lines, installing cobblestones or pavers across the roadway, and introducing vertical elements within the motorists' field of vision that mark the Promenade intersections. Bollards, light fixtures, or landscape elements can be brought closer towards the driving lanes at the intersection to provide these cues.

The visualization depicts light fixtures being brought forward, close to the curb, as well as palms located forward of the building façade.

The finished grade of the crosswalks across the intersections should also be matched to the elevation of the Promenade walkway and sidewalks at the avenues. This will in effect create a traffic-calming device called a "speed table," indicating to the motorists that they are crossing a pedestrian space of primary importance.

#### 4.7 Shade

The Promenade is fairly narrow, and shade in such narrow paseos is traditionally provided by buildings rather than regularly-spaced street trees. Because regularly-aligned trees planted within the promenade would further reduce the size of the pedestrian realm, the decision was made not to include regularly-aligned shade trees as part of the design, but instead to incorporate single trees in a few locations only. Shade is then to be provided by various means:

**Buildings-** As new buildings are developed along the promenade, they will most likely be taller than the existing structures. The height will assist in providing shade.

**Projecting elements -** Elements which extend forward of the façade such as marquees, awnings, galleries, colonnades, balconies, and roof hangs are to be incorporated in new and renovated buildings.

**Canopies over the Promenade -** Canopies can provide shade but can also block views to signage and buildings. Any future canopy design that is implemented must be sensitive to this effect. This concept was explored by the design team and City staff. The conclusion was that the canopies are an idea that should be held in reserve if needed at some

future time, but the canopies are not required in the initial phases of the Promenade.

#### 4.8 Lighting

Night lighting can add a sparkling, dazzling effect to the Promenade. This will be created by light emanating from inside restaurants and shops, electrified signage, up-lighting of banners, exterior doorway lighting, and of the strategically-placed individual trees and palms. Once the nighttime destinations on the Promenade are up and running and have done enough to brighten the scene, light fixtures will actually provide relatively little of the lighting.

In the early stages of development, however, it is likely that there will not be the necessary continuity of lighting from building to building along the Promenade. Light fixtures, free standing or attached to buildings, will therefore be necessary to provide initial light for visibility and safety. As more buildings along the Promenade are developed or renovated, they can add more lighting elements, elevating light levels further and bringing more visual interest and continuity.

#### 4.9 Two Operable "Front" Doors for Each Business

To keep 10th and 11th Street active,

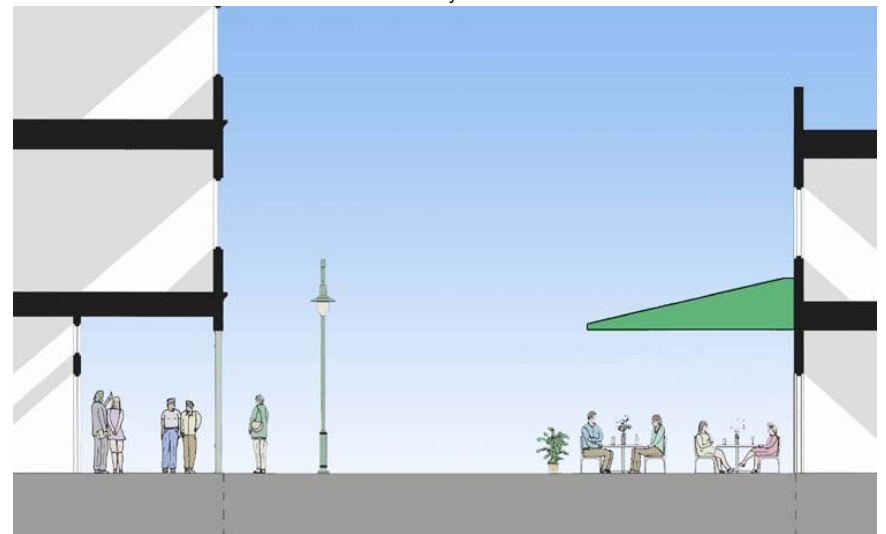
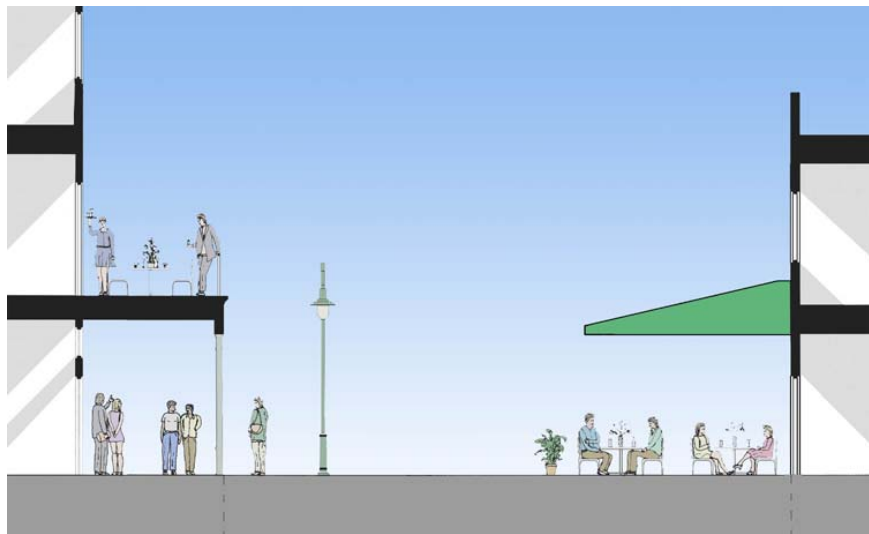
buildings adjacent to the Promenade should have operable doors facing both onto the Promenade as well as onto the adjacent street.

Depicted is a proposal for a canopy over the Promenade that will not overly block views to the signage and buildings. The canopy is depicted as intermittent triangular segments. They can be attached to existing light fixtures, buildings, or decorative poles. The canopy would be composed of grids of "harlequin panels" strung together with space between each panel. The panels would create a semi-transparent effect of dappled shade. The alternating triangular segments and openings would provide openness to the sky but also provide relief on the hottest days.



## 5.0 The Plan

The hypothetical build-out plan depicts the variety of possible outdoor spaces along the Promenade and explains how well-designed buildings and façades are necessary to create interesting pedestrian spaces. This plan is labeled "hypothetical" as the intent is to depict the variety of spaces, rather than focus on the exact location for each one at this time. The concept is for the SEOPW CRA to work interactively with property owners and investors to incorporate the concepts as projects unfold, to avoid setting off an inordinate amount of real estate speculation. Ideally, the goal is to create one focal point or special space within each block, with cross block passages to connect each block along the Promenade with adjoining destinations to the north and south. The following pages are the description of the public space envisioned, the necessary details, and how each one can be different from the other.



Shade for pedestrians can be accommodated with awnings, colonnades, and arcades. Second and third floor terraces add visual variety as well as natural surveillance to the Promenade.

## The Plan

The Promenade is designed as a series of spaces. From west to east, there is the gateway to the Promenade on NW 1st Avenue. The next block contains a paseo (passageway) and parking garage. To the west is a small plaza, then the eastern gateway to the

Promenade, and finally, Bicentennial Park. This sequence of spaces forms the central path of the Promenade. Branching off from the central path will be improved sidewalks and new mid-block paseos linking pedestrians to parking garages, parking under I-395,

Bicentennial Park, the Performing Arts Center, and future cultural activities and businesses focused around the Lyric Theater. All the pedestrian walkways will be properly lighted, and shaded with tree canopy or building façade elements.





## 5.1 Block One: Northwest 1st Avenue to North Miami Avenue

Block One is the block furthest west and is likely to become the most dramatic public space in the linear sequence. The plaza is envisioned as the western anchor of the Promenade, a gateway to Overtown, and a counterpart to the eastern anchor of Bicentennial Park. Located on the northwest corner of this block, two new buildings are to face and define the plaza. These new buildings, whether modern or traditional in architecture, are to respond to the climate in the Miami tradition of colonnades and second floor galleries. The façades of the buildings which define this plaza must be memorable and engaging, as they define the image of this civic space.

There has been discussion about creating a signature "Miami Multimedia Tower" (or 'media tower' for short) in some location within the Overtown / Park West / Omni area. One possible location- among several- for the media tower is on the southern side of the Block One plaza. For discussion purposes, this Promenade plan includes illustrations of one design concept and location for the media tower, although it is recognized that the concept will evolve and that the location has not been finalized. Ideally, however, the media tower would be located where it can serve as a focal feature for the

energetic, glittering scene of the Promenade and provide a landmark for wayfinding as pedestrians move west from Bicentennial Park and the cultural destinations of Biscayne Boulevard.

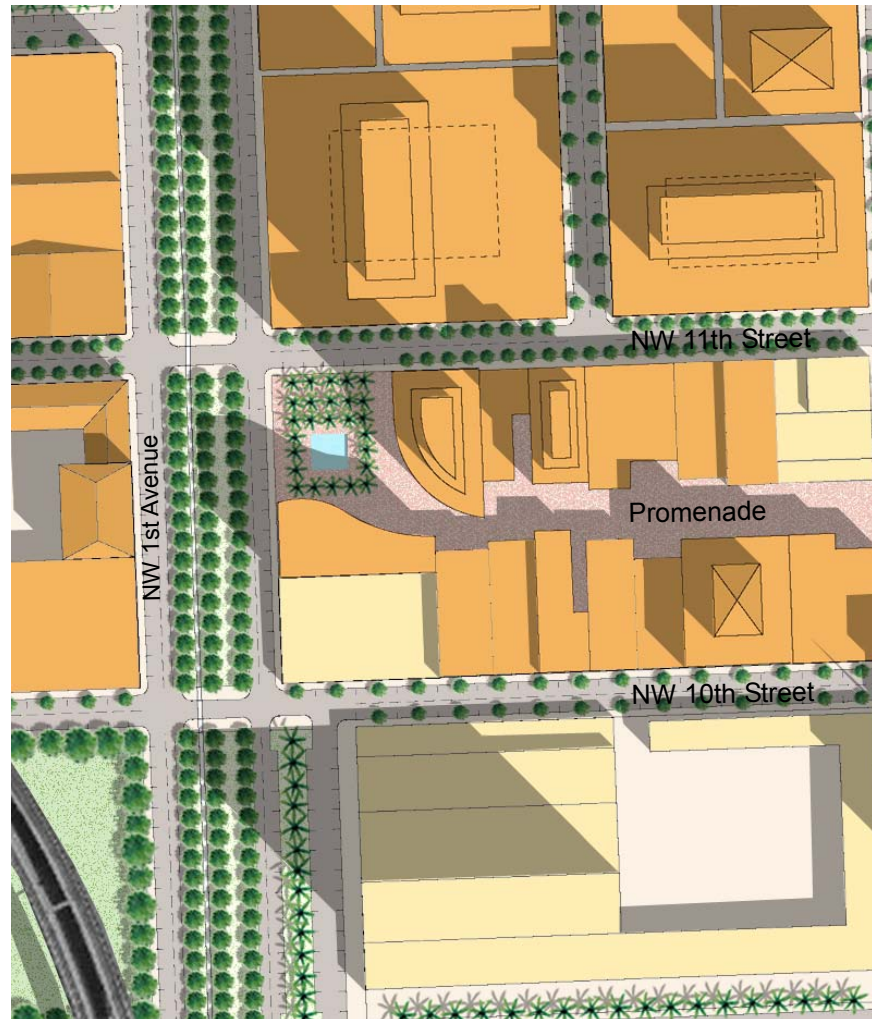
With its unique image, the tower has the potential to be an iconic part of the cityscape and Miami's popular culture. Therefore, the design and detailing are extremely important. In these images, the tower is depicted as having three distinct components. The base would have several floors of habitable space for restaurants, retail, and possibly office uses. The tower element would be an open superstructure designed for "plugged-in" electrified advertising panels. The top should have a geometrically distinct, symbolic, or sculptural component, whether this is accomplished with advertising iconography or artwork. One further possibility for the top level is to include an observation platform for public.

To create interest, animate the space, and encourage people to use the plaza, a water element and figurative sculpture is proposed for the plaza. Once the buildings that will define the space are being developed, a competition should be held for the design of the water element and sculpture, which should be evocative of multicultural Miami in some way.

Miami Multimedia Tower







## 5.2 Block Two: North Miami Avenue to Northeast 1st Avenue

This second block is depicted with two elements of the plan, a mid-block plaza and parking structure. This should add an opportunity for parking that can be used by the museum visitors to Bicentennial Park, as well as visitors to the Entertainment District.

The mid-block plaza should be well proportioned, no longer than a 1:2 proportional dimension (no longer than twice as long as it is wide). If the plaza becomes too long it will not be perceived as a unified space. This plaza is intentionally envisioned as open and flexible, rather than occupied by permanent fixtures, so it can be transformed for a variety of uses. These include:

1. A performance space that can accommodate a small temporary stage for formal and informal concerts and shows.
2. Events including art festivals, open air market, or food vending.
3. A gathering space / meeting point for visitors from other destinations along the Promenade.
4. Outdoor dining for any adjacent restaurants, night clubs, or cafes.

### The Design of the New Building with Parking Structure

Rather than create a typical parking structure, the recommendation is to create a new signature building- an exciting building that sets a high architectural benchmark in the Entertainment District, and happens to have additional parking capacity.

With a "liner building" approach rather than a plain garage, this would be a building that has habitable space on the façade, helping to define safe and inviting destinations onto the streets and plaza. The topmost floors, depicted in the illustrations as penthouse villas, would offer a unique residential building type; these can be designed as townhouses, offering multi-level and/or loft-like spaces which provide privacy while affording private outdoor space. The units on the upper floors would also result in a varied silhouette on the roofline, improving the overall massing of the building.

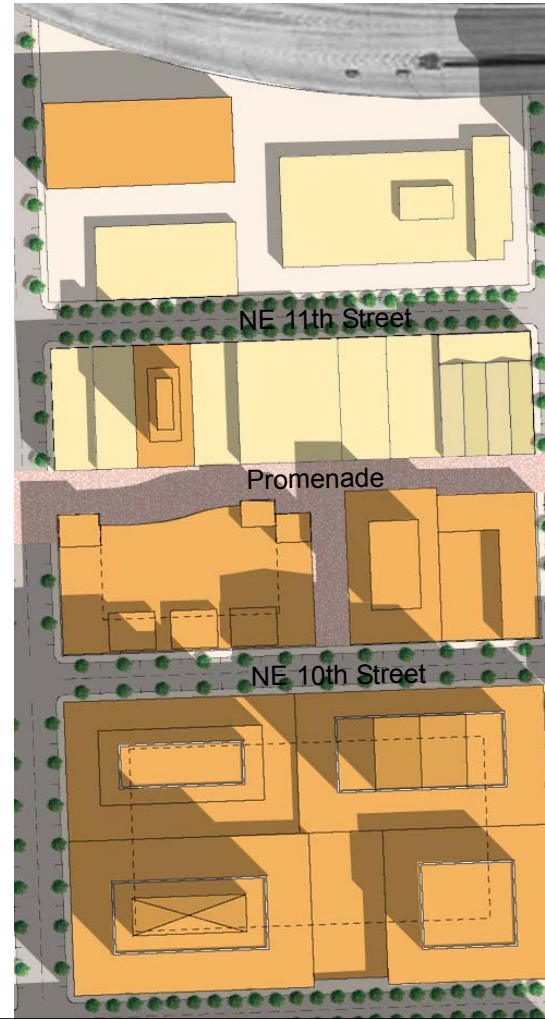
Within the space constraints, the liner building with parking structure can accom-

modate more parking spaces by utilizing a portion of the Promenade right-of-way, resulting in a site of adequate depth to create habitable space along three sides of the building. The plaza, Miami Avenue, and 10th Street side of the building must have real doors and windows. Advertising panels would wrap the fourth side facing the Promenade. It is proposed that the Promenade side of the building should be curved and deflected to improve the visual prominence to the advertising panels. The curved side will direct and shape the linear vista along the Promenade, creating a distinctive experience on the sidewalk. Storefront uses (such as retail, restaurants, or drinking establishments) should be located on the first floor to maintain continuity of destinations along the Promenade. The plaza façade could have a second floor gallery. This would offer the desirable opportunity for second floor restaurants and bars, for people-watching and enjoying performances on the plaza. This space would be set up and provisioned for such uses at some point; note, however, that

may be difficult to coax tenants and customers to utilize second floor spaces until a critical mass of success has been achieved along the Promenade and in Park West as a whole, so this concept is not essential for an initial phase.

Egress for vehicles from the parking structure is to be located on 10th Street.

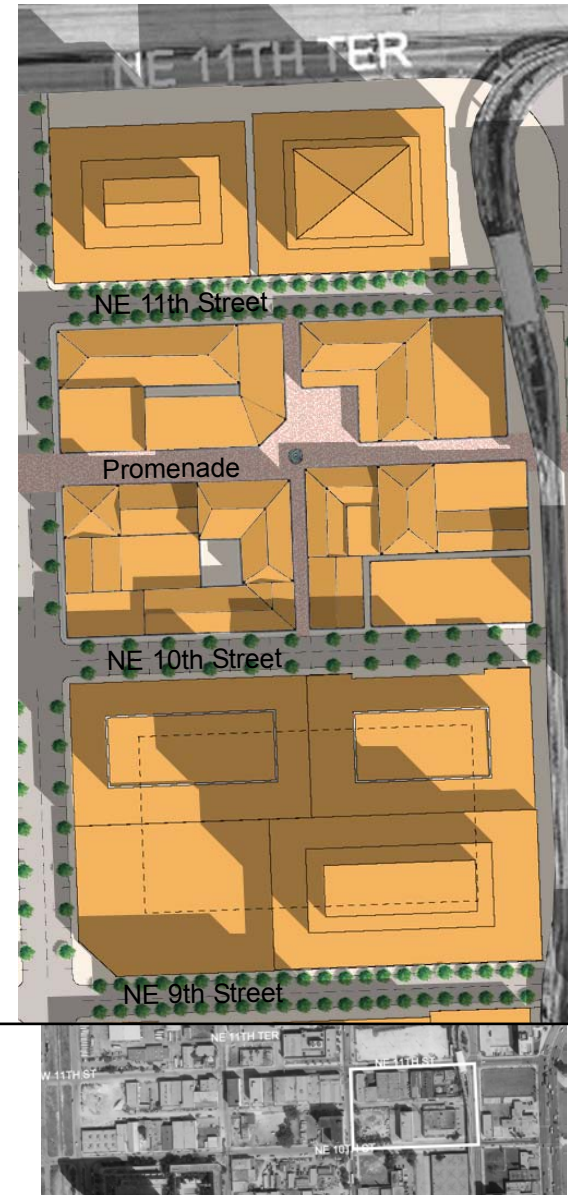
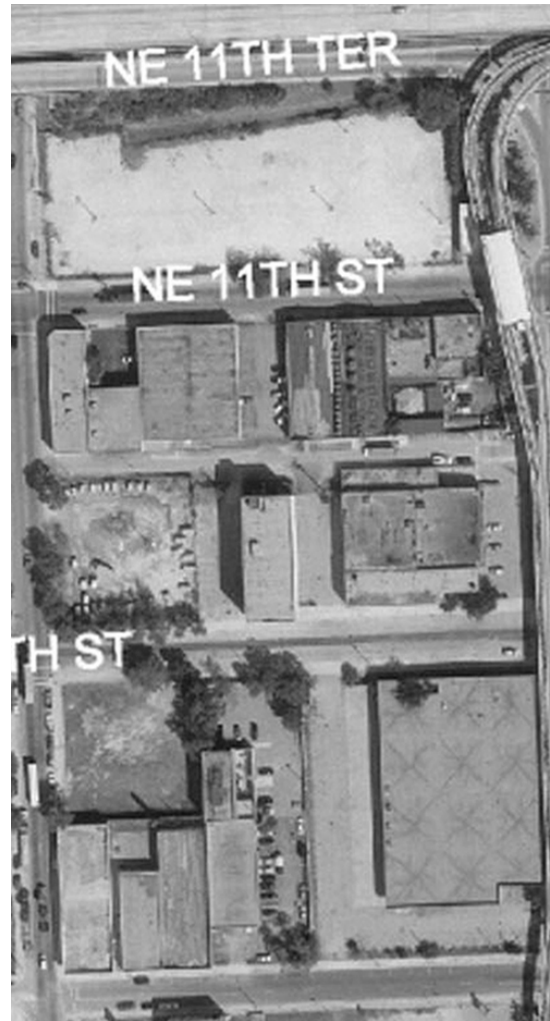
As described above, developing a "lined" parking structure on the Promenade may require additional parcel depth. Lot depths along the Promenade are shallow by the contemporary standards for parking structures. Innovative parking designs will need to be evaluated to fit in the lot sizes available so that both adequate parking and liner building depth is accommodated. Cooperation with the Miami Parking Authority in this regard is highly recommended. Created cooperation between regulators and developers is invaluable; otherwise, this building may end up looking like a plain parking structure that would detract from the potential of the Promenade, the plaza, and the streets.

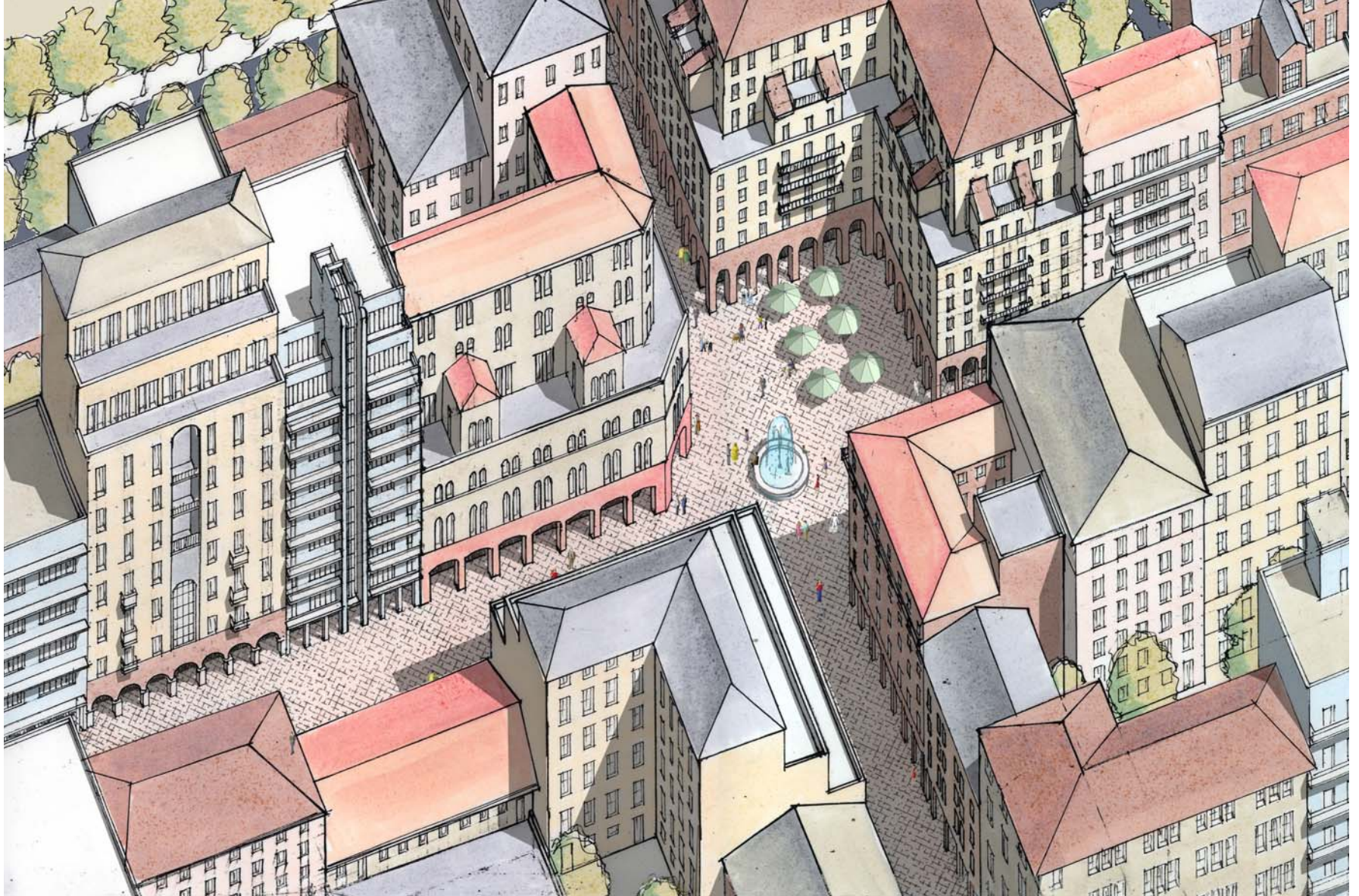




### 5.3 Block Three: Northeast 1st Avenue to Northeast 2nd Avenue

The third block is depicted with a small plaza; this piazzeta can be created by buildings stepping back and defining the space. The difference between a small plaza and the mid-block plaza is this intimate plaza does not extend to the adjacent street. They are connected to the adjacent streets by pedestrian passages or paseos that can be narrower than the Promenade. These kinds of intimate courtyards are successful when the buildings and spaces are rich with detail at the pedestrian level. These smaller spaces offer are often desirable locations for destination businesses which are not necessarily reliant on high visibility, such as upscale restaurants and art galleries.





Possible character for buildings along the Promenade following new guidelines. Adding a plaza in a mid-block location can provide flexibility for outdoor events.

## 5.4 Block Four: Northeast 2nd Avenue to Biscayne Boulevard

Block Four is a half-block in depth compared to the other blocks. This segment is the key parcel which connects Bicentennial Park; it is also under private ownership. The Special Area Plan recommends that the City and CRA work with the property owners to enable this final key linkage to complete the Promenade's connection to Biscayne Boulevard and Bicentennial Park.

This linkage can be accomplished in several ways:

### 1. Purchasing the property

Gustafson & Priore has recommended purchasing the northern half of the block. Through this approach the CRA can opt to be a partner in the development of the parcel as well as ensure the creation of this segment of the Promenade.

### 2. "Coordination" with the property owner

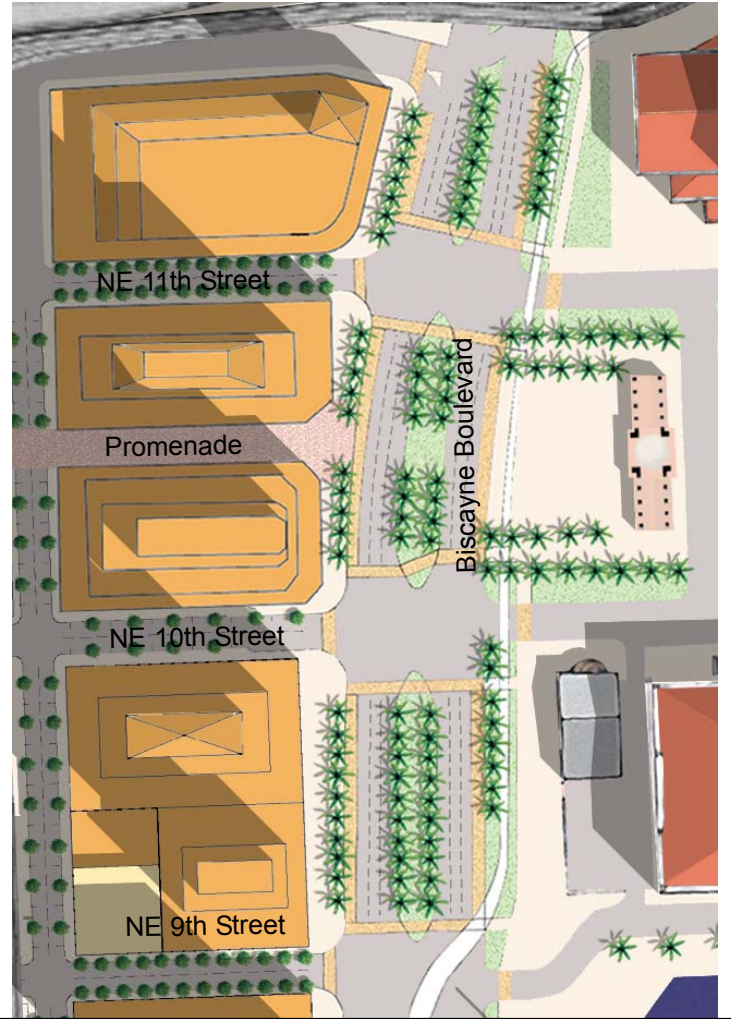
The CRA can work with the property owner and development entity to explore maintaining the east-west parcel as an open-air pedestrian environment. Under this approach the block would be developed as two separate

buildings. The benefit to the property owner is to create additional street level frontage which has retail, restaurant, and outdoor dining potential. The CRA may apply financial incentives or other inducements to encourage this open-air easement.

### 3. Maintaining an interior pedestrian gallery

If the other two options are not feasible, a shopping arcade or gallery which maintains a pedestrian connection through the interior of buildings is a third possibility. Such spaces have precedents in many world cities and in Miami, including the Seybold Arcade in downtown. This space could be the subject of a pedestrian easement conveyed to the City by the developer. Such interior connections offer some retail potential and are often designed as spectacular spaces. Wide openings with tall ceilings would maximize the visual connections from one side to the other by inviting light in through the base of the building, especially along Biscayne Boulevard.





## 6.0 Implementation

The vision for the Promenade is illustrated in this Special Area Plan through preliminary three-dimensional renderings and computer visualizations. The images are intended to capture the viewer's imagination, to create the sense that the vision for the Promenade can be made real. However, more refinement of the concepts will occur during implementation. Identified below are the necessary components and steps to assure the continued development of the District.

### 6.1 Adopt the Promenade Special Area Plan in Concept.

The City of Miami and the CRA should pass an Adoption-in-Concept resolution giving the Special Area Plan (SAP) official standing. The SAP should be endorsed as a supplement to the City's other planning instruments, such as the zoning regulations, neighborhood plans, and Comprehensive Plan. Adopting this plan sends an important message to the development community, property owners, and residents that the political decisionmakers support this plan. It also provides the Planning Department staff with a sound basis for instructing applicants to meet the goals of this Plan, even while revisions to existing regulations are underway. Applicants seeking to fulfill the vision in the

early months may need extra assistance, technical support, and an extra level of expedited service to offset the likely inconveniences of zoning variances, special exceptions, and other considerations.

### 6.2 Adopt the Proposed Overtown / Park West Regulating Plan and Code.

Land use, building heights, and building form are to be refined in this new Code so as to enable implementation of the Plan. Currently, lots adjacent to the Promenade are approximately 100 feet deep; this can be a challenging functional constraint for some contemporary building types. In the zoning regulation, the low Maximum Building Footprint of 40% (the percentage of the property which can be occupied by a building), and the Floor Area Ratio of 1.72 (the amount of square footage that is buildable, based on the size of the lot), drive the need to acquire more land to create adequate developable square footage and make a new building feasible. Similarly, the minimum parking requirements are inappropriate given the unique nature of this urban-core district. The Maximum Building Footprint, parking requirements, and Floor Area Ratio rules should be revised to:

- Allow for adaptive re-use of existing buildings;

- Allow for smaller parcels and projects to be developed; and
- Relieve small businesses from the requirement for large amounts of parking onsite.

Specific changes recommended for the Regulating Plan and Code include:

- Increased Maximum Building Footprints;
- Re-examined Floor Area Ratios (possibly eliminated, if feasible);
- Decreased or eliminated minimum parking requirements;
- Decreased Open Space requirements; and
- Building envelope standards, including step-backs for towers.

The new Regulating Plan and Code should describe:

- The building-to-street relationship (how buildings should face the street at the ground level). This includes elements such as distance between operable doors, percentage of glazing, and the use of projecting elements to provide shade, such as colonnades and awnings.
- Building envelope (how the mass of the building is adjusted to preserve access to

light and prevailing breezes, and optimize views, while not unduly limiting development potential).

- Architectural elements (the basic architectural features to create functional responses to climate, visual interest, and urban fit for buildings).
- A mixture of uses and improved parking policy (for example, removing on-street parking requirements for businesses located at the street level to encourage shopfronts to occur below residential or office structures).

The overall benefit of the Regulating Plan and Code will be to create a new balance of flexibility and predictability, allowing developers at varying scales to build a range of appropriate building types that add value to the whole.

### **6.3 Manage Parking Needs**

In car-dependent cities such as Miami, parking is expected to be provided free or very cheaply in most of the region and the development industry typically will not build without having parking readily adjacent. As a result, every project or building developed may attempt to provide for their parking on-

site with a parking structure or big surface parking lot. During this interim phase, the urge by property owners to "solve" parking needs on their own will be an impediment to development, by making small investments less feasible than big ones. Worse, where redevelopment does occur, the wrong kind of parking facilities (such as surface lots along streets or blank garages) can inhibit pedestrian comfort and create a still more blighted appearance, working against the overall neighborhood redevelopment goal.

Such a parcel by parcel approach to parking is especially unrealistic on the shallow properties adjacent to the Promenade, where a system-wide method to parking is essential.

In the future, as shared parking facilities are built and the area becomes more developed, livable, interesting, and walkable, with daily needs and services present, parking expectations will likely evolve. Residents may rely less upon single-occupant automobile trips, own fewer cars per household, park fewer cars, and be willing to park farther from their workplaces, homes, or destinations. Developers in desirable locations will eventually begin to build with lower parking ratios to improve the efficiency and optimize the income-producing forms of their buildings.

Parking policy for the Promenade should therefore include pro-actively providing new parking supplies as part of the overall management of the area, as well as strategic regulatory reform to promote redevelopment investment. There are two parts to this recommended policy:

1. Build shared parking facilities to better address peak parking needs. The CRA can play a proactive role, through private-public partnerships, in creating shared parking facilities.
2. Provide on-street parking on all new and improved streets.

Additional parking structures, lined with habitable buildings, should be evenly distributed throughout the District rather than concentrated right on the Promenade.

### **6.4 Adopt an Outdoor Dining Ordinance**

An Outdoor Dining Ordinance should be created specifically for the Promenade. The ordinance should be set up to encourage outdoor dining during this pioneering phase and as an ongoing feature of the Promenade. Incentives would include:

1. No permitting fee for at least the first two years.
2. No initial leasing fee for the use of the Promenade.

3. No additional restroom facilities required for the additional outdoor seating that is created.
4. Allow for tables to spillover along the Promenade in front of adjacent buildings, if that adjacent property is not providing outdoor dining, provided the neighboring tenant and management agrees.

A clear path at least twelve (12) feet in width through the center of the Promenade must be maintained. Outdoor dining should only occur within 25 feet of the Promenade façade of the buildings (The width of the Promenade varies from approximately 35 to 50 feet), or within courtyards or plazas adjacent to the Promenade. The specific dimension of the footprint for outdoor dining areas should be reviewed and established on a case by case basis by the Department of Planning & Zoning.

### **6.5 Officially Adopt Policies for the Miami Multimedia Tower Project**

Adopt the Miami Multimedia Tower Project as a concept to be developed in a creative public-private partnership. Define the terms and logistics that will be required to undertake the project, including the financial roles of the City (if any) and CRA. If necessary, conduct a Request for Proposals from

private entities to determine the best location, design, and business deal for the Multimedia Tower. Devise and adopt an ordinance that will specifically address the physical parameters under which media elements and signage are to be permitted on the media tower.

### **6.6 Explore Adopting an Open Container Ordinance**

As a catalyst to encourage proprietors to provide entrances and operative doors facing the Promenade, the City and CRA should explore an Open Container Ordinance applicable solely on the Promenade. Open containers with alcoholic beverages would only be allowed on the Promenade itself and in adjacent courtyards or plazas between 10th and 11th Street, encouraging movement through the Promenade rather than the main entrances of the facilities on 11th Street or 10th Street. If necessary, the Ordinance can be set up to allow open containers only in specified locations and during certain times of day.

### **6.7 Construct Shared Trash Centers for Managing Waste**

The Promenade design converts the former alley, typically the location for services, into part of the public realm. The collection

of trash therefore needs to be managed more carefully than in the past. As the Entertainment District becomes more active, the amount of waste- and the speed with which waste is generated- will grow much greater. Shared trash handling centers will become a necessity. A shared trash center is basically a large trash compactor approximately the size of few parking spaces, housed in a space of approximately 20 by 40 feet. Facilities of this kind are often embedded into part of a building, so all that is visible at street level is a door. The system compacts, chemically treats, and cools the waste so that it does not generate bacteria or odor. Such systems are typically used by a number of businesses cooperatively.

The Promenade will most likely need one shared trash center per block. The CRA can assist in implementing the system, and as the district matures, the management could also be handled by a merchants association or similar entity. The shared trash centers should be carefully located so they are not visually apparent and do not detract from the continuity of façades.

### **6.8 Produce a Demonstration Project**

A high-quality demonstration project should be organized and implemented within two calendar years of the adoption of this

Special Area Plan. This demonstration project should aim to capture the imaginations of investors and motivate financial support. The demonstration project should include a building project (whether rehabilitation or new development), and, if possible, one special outdoor space along the Promenade. The demonstration project could include a parking shuttle transit vehicle to circulate with the SEOPW CRA district connecting the Promenade and shared parking spaces. It should ideally make evident all the elements of urban design described in the Special Area Plan. This demonstration project can utilize the potential private-public partnerships as described in the CRA Plan Update and Park West Economic Strategy. It should occur at an area where a segment of building frontage is built as part of a street improvement for new development or redevelopment. It is imperative that these initial projects are of the highest quality. The first parcels developed and redeveloped under the Plan will set the standard for all subsequent efforts.

### **6.9 Create a Business Improvement District (BID)**

Transforming large segments of the street to follow the vision needs to occur through a multi-phase capital improvement project and a revitalization strategy carried

out on many fronts. This will require the City, Community Redevelopment Agency (CRA), development industry, as well as the property owners and business owners to work together.

As the district develops, new businesses and buildings will be brought online; the day-to-day management and ongoing maintenance will need to be addressed in a timely manner. A Business Improvement District (BID) can be set up to ensure and improve the prospects for the entire business community along the Promenade. A BID can generate strong leadership from among the business owners themselves, charged with promoting development and redevelopment along the corridor while stimulating economic growth and prosperity. Thus set apart, the district would generate its own, highly-focused projects paid for by tax or assessment based revenue resulting from the creation of a self-taxing or assessment district. A portion of property taxes or assessments could also be used for directly beneficial activities (maintenance, security, marketing, business recruitment, festivals, community gardens, market initiatives). The BID can create a culture of overall management for the area, coordinating the efforts of the many involved property owners in marketing, operating, promoting, and enhancing the District.



## 7.0 Design Guidelines

Very little has changed in the appearance of Park West over the last several years. There are some simple actions that property owners can undertake to improve the overall image of the neighborhood. More significantly, real estate transactions have been taking place, suggesting that there is momentum by private interests to substantially redevelop Park West. Much of this activity is centered on 11th Street in the City's burgeoning Entertainment District, thus invigorating The Promenade.

Redevelopment to-date has taken place mostly by renovating existing buildings. This is unusual given that most of the buildings are one or two stories in height, with an allowable height of approximately 12 stories. The only saved portion of the original buildings is the shell of the building. One night club has dug down below the water table to create an expensive water-proofed multi-story basement. After examining the zoning code, the reason for this is quite clear.

Properties along the Promenade have the C-1 zoning classification. This limits the footprint of the building to 40% of the gross lot size (the net lot size plus one half of the roadway in front). Many of the existing buildings occupy close to 100% of the net lot size. So to tear down an old building and put up a new one, a developer will have to build a two or three story building to get the same square footage as currently exists at ground level.

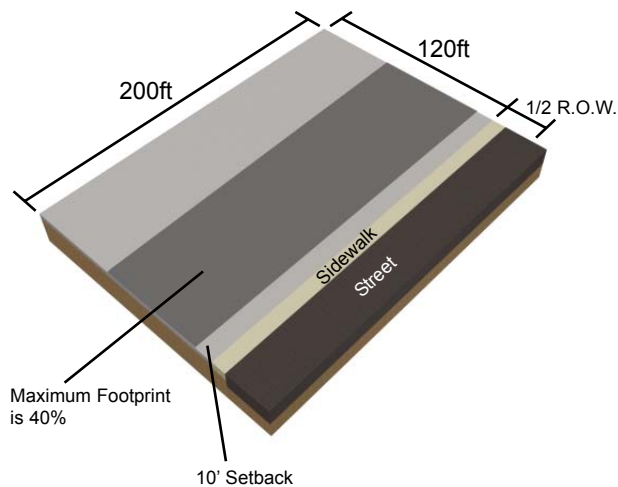
This section of the Promenade Special Area Plan describes new rules and guidelines for properties along the Promenade in Park West. The first sub-section is a brief study of the existing zoning regulations and identifies the reasons for modifying the zoning. The next sub-section provides new rules to be used as guidelines for redevelopment. That is followed by guidelines for property owners who are making minimal improvements to their properties.

## 7.1 Increasing Development Potential and Variations - Why It Is Necessary for Redevelopment

The current zoning regulations are limiting redevelopment activity in Park West. The development intensity has to be increased from the current limits. To understand why, it is necessary to first look at the current zoning regulations and their impact on buildings in Park West.

### Current Zoning Analysis

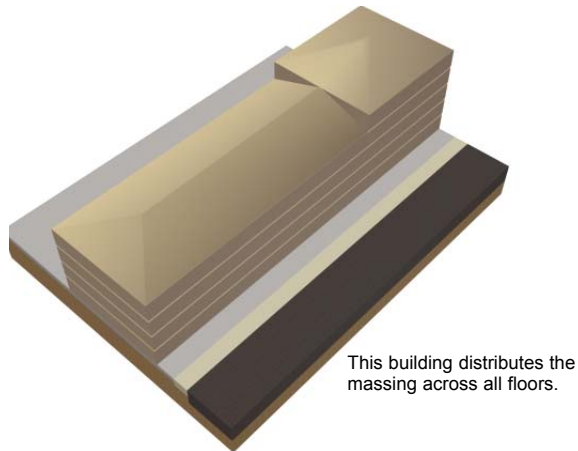
The current zoning (C-1) for properties along the Promenade allows many possible outcomes for future construction. To study the build-out potential of a given parcel of land, certain assumptions have to be made. This analysis uses a parcel of 120 feet deep by 200 feet wide. The gross square footage for calculations includes half of the 50 foot wide street right-of-way in front of it. So for our example, our gross square footage will be  $(120 + 25) \times 200 = 29,000$  square feet (sf). Assuming that our intention of a continuous street wall is the goal, we have to figure out a building footprint for the site.



- There is a required 10 foot setback in the front.
- There are no side setbacks unless along a street or different landuse.
- The maximum footprint allowed is 40% of gross lot size. In this example, it is 11,600sf. ( $.4 \times 29,000\text{sf} = 11,600\text{sf}$ )
- The preferred length of the building along the street edge equals the lot width of 200 feet.
- The depth of the building works out to be 58 feet by dividing the maximum coverage by the 200 feet front dimension. ( $11,600\text{sf} / 200 \text{ feet} = 58 \text{ feet}$ )



To determine the maximum height we have to look at the floor area ratio (FAR); the zoning code sets this as the maximum buildable square footage that can be built on the property.



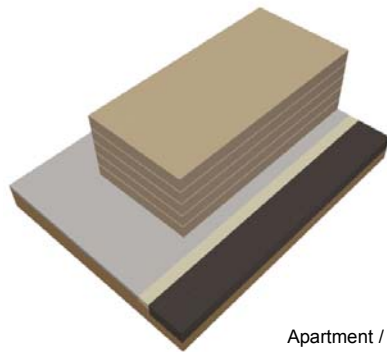
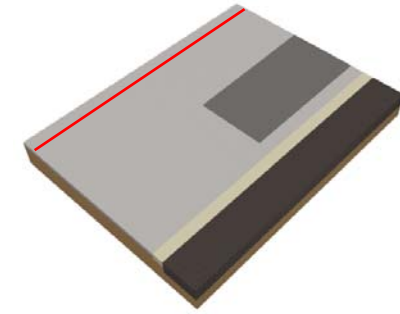
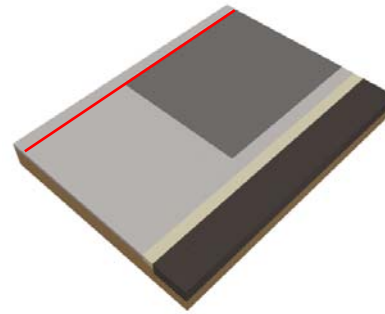
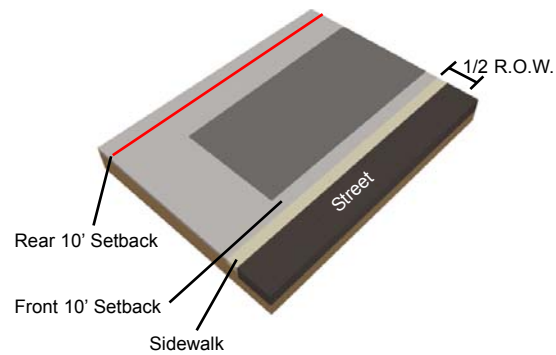
- FAR = 1.72 [From zoning code, C-1]
- Maximum Square Footage allowed for building =  $1.72 \text{ [FAR]} \times 29,500 \text{ [gross sf]}$   
= 50,740sf
- Number of stories =  $50,740\text{sf} / 11,600\text{sf}$   
= 4.37 stories
- In C-1 Zoning, there is no height limit, except that there are setbacks above 120 ft., typically 11 to 12 stories.

*What does this analysis show us?*

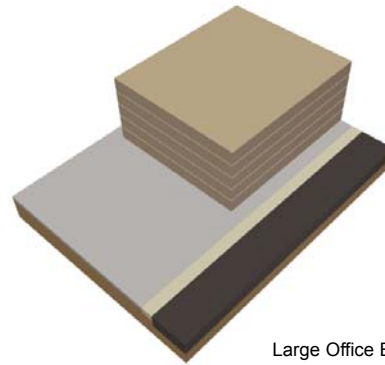
1. To build out the street edges, current zoning FAR limits the height to about 5 stories, if floor plates are consistent in size.
2. If the goal is to build out the street edge and maximize the height, the resulting building depth at 58 ft. is not very efficient for residential and commercial uses. Small scale retail and boutique stores typically have a 50-60 ft. depth, which works well with this scenario, but larger stores, such as a grocery, pharmacy/drug store and department stores require a deeper building floor plate.
3. The scenario illustrated is not likely, given the narrow building depth. More likely, redevelopment will require a larger assembly of land. The necessity of requiring large parcels perhaps explains why Park West has been slow to redevelop in the 20 plus year history of the CRA. Existing ownership is in smaller parcels.

## Variations of the Building Footprint

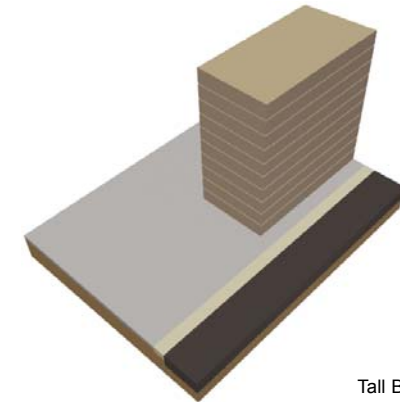
To get an efficient footprint, the continuous street wall will have to be sacrificed. C-1 Zoning allows the placement of a building to be anywhere as long as it is behind the setback lines.



Apartment / Condo



Large Office Building

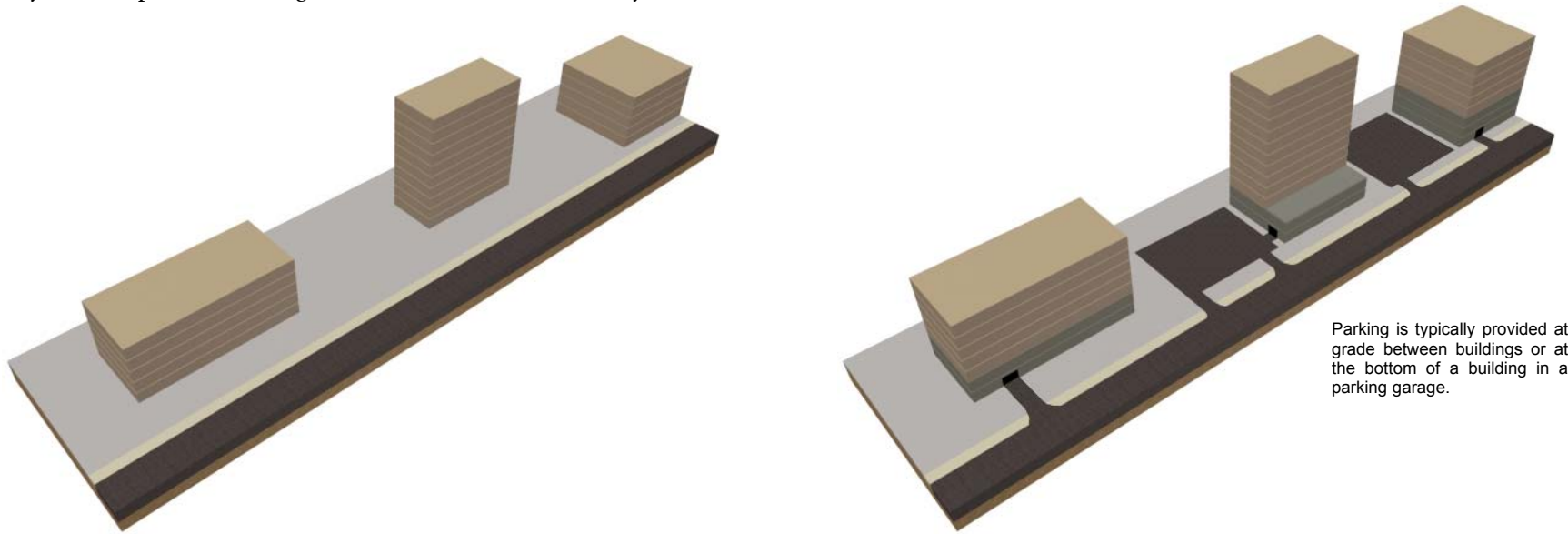


Tall Building

- There is a 10' front setback.
- The maximum footprint allowed is 40% or 11,600 sf.
- Apartment/Condo buildings are typically 75 feet deep yielding a footprint with 155 feet of frontage.
- Large office buildings are typically 100 to 120 feet deep. The setbacks of 10 ft. in the front and 10 ft. in the rear cause the maximum depth to be 100 feet; so the footprint will be 116 ft. wide by 100 ft. deep.
- To maximize the height with a small but realistic footprint, one could build a 10-story building with a footprint of 50' x 100'.

## Forming a Street with these Buildings

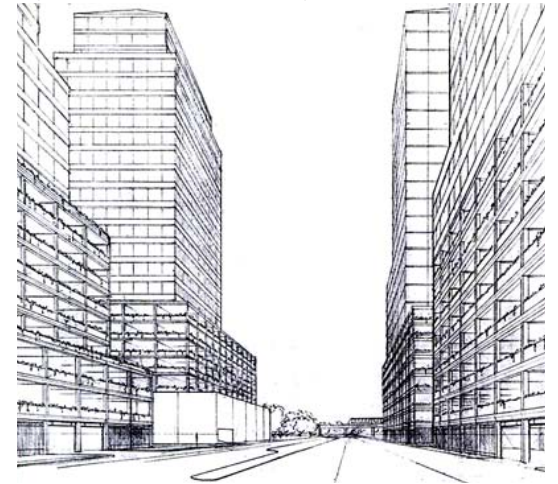
When you line up these buildings on a street, the end result may look like:



This configuration is not the best approach to encourage pedestrian movement.

- The 10' setback pushes the buildings away from the sidewalk.
- The buildings are not required to align along the street nor have their primary entrance on the street.
- Gaps between buildings are not recommended for retail streets because it causes some shoppers to stop and turn around.

Parking garages are typically constructed at the bottom levels. Streets can be lined with parking garages on both sides, making the view unpleasant and boring.



## 7.2 Guidelines for Redevelopment

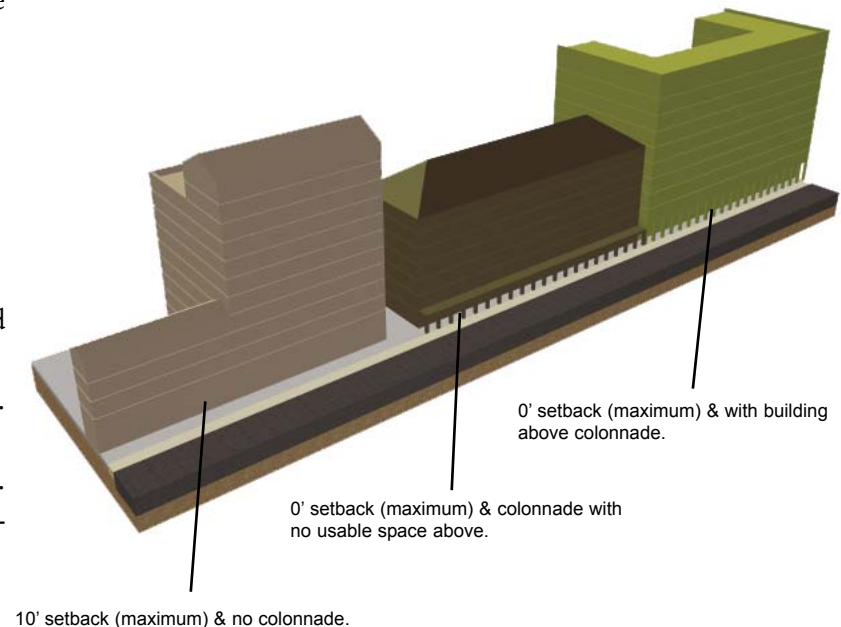
The following rules describe a new urban form for Park West and are divided into 5 sections:

- Building Disposition - identifies setbacks, lot coverage, parking locations, and building footprints
- Building Configuration - identifies heights, building elements, and components
- Building Type Standards / Land uses - identifies permitted land uses
- Parking Standards - identifies minimum requirements and configurations
- Additional Design Standards - identifies design aspects to the exterior of buildings

These rules have been prepared in such a way as to form the basis for new zoning regulations for Park West and Downtown Overtown as described in the SEOPW CRA 2003 Redevelopment Plan. Until such time as new zoning regulations are adopted by the City of Miami, these rules should serve as guidelines for new development and may serve the Department of Planning and Zoning as a basis for granting variances to the existing zoning rules.

The intent of these new rules is to provide the following objectives:

1. Sidewalks along streets are to be faced with doors and windows to habitable space such as stores, offices, or residential units, for the almost the entire length of the block.
2. Limitations to building footprint, massing, and height that bias large scale development are to be replaced with rules that can also benefit the small property owner.
3. Development intensity is to be increased to provide a stronger economic base for goods and services in the neighborhood.
4. Parking garages are to be shielded from view from the street, and separated from the street by habitable space, or by a separate habitable building.
5. Open space requirements may be transferred to roof tops or upper story decks.
6. The minimum height of new construction is to be greater than one story.
7. Variation in height of buildings is to be increased to avoid a flat top skyline.
8. A lively pedestrian environment (The Promenade) and other pedestrian oriented corridors.



## Building Disposition

### Building setbacks from property line

This Special Area Plan uses setbacks and build-to lines. A setback is the distance inside a property line that a building wall must sit at or behind. A build-to line is a distance inside a property line that a building wall must sit at.

Street Front: There are two options:

1. Build-to line at ten (10) feet if there is no colonnade, or
2. Build-to line at zero (0) feet with a colonnade.

Buildings on corners should apply the street frontage requirements to the side street frontage as well.

Side build-to line: for the first twenty (20) feet of building depth, zero (0) ft. Min.

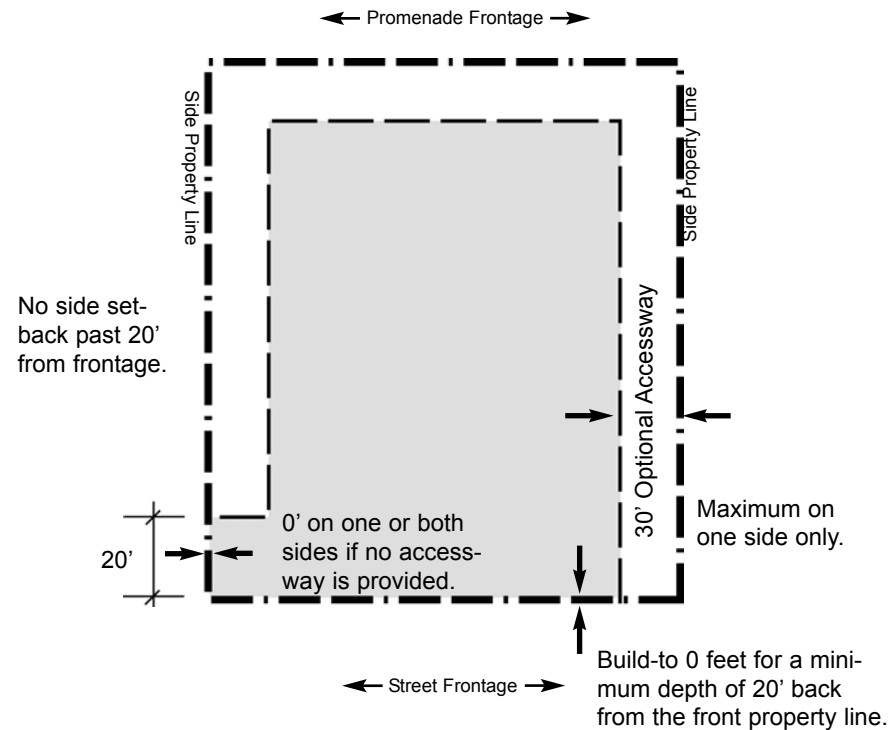
Side build-to line: for the remainder of the building depth: none. This is equivalent to a zero (0) foot side setback; the rear portion of a building may be any width.

Side Setback exception:

On one side only, the front portion of a building may set back up to a maximum of thirty (30) feet to allow for vehicular and pedestrian access to the rear of a property or to a garage. Use a twenty-four (24) foot build-to line if only providing two-way vehicular access. Side accessways on neighboring properties should be shared to avoid large "gaps" between buildings, provided there is a written agreement between property owners.

Promenade Front: Eighteen (18) feet from the center line of the Promenade or zero (0)

feet from the rear property line, whichever distance is the greatest from the centerline of the Promenade. The centerline of the Promenade is at the midpoint of the greatest width of the R.O.W. for any particular block.



### Frontage and primary entrances

Façades shall be built parallel to frontage lines along 100% of the distance between side setbacks. In the absence of a building or accessway along the remainder of the frontage, a streetwall co-planar with the façade shall be built.

Buildings shall have their principal entrances on a frontage line. Buildings along the Promenade shall have an additional entrance facing the Promenade.

### Usable Open Space and Lot Coverage

The maximum lot coverage determines the size of the building footprint. Any rules regarding the usable open space or green space can also affect the building footprint. Usable Open Space is a landscaped or paved area or areas available to the residents/occupants of the building or the general public. Green space is a pervious landscaped area at ground level. Currently for properties in C-1 Zoning, properties have a maximum footprint of 40% of the gross lot area, and a green space requirement of 10% of the gross lot area.

This Special Area Plan suggests changes from the current rules. Lot coverage should be allowed to be at 100% of the area between setbacks and build-to lines. The green space requirement should be replaced with a usable open space requirement that may occur at ground level or on roof-top terraces. The reason for this change in terminology is that usable open space may be hardscaped and green space is defined as a pervious area at ground level. 100% of the usable open space should be

allowed for semi-private usage; however, only in common areas of the building. Areas inside balconies, terraces, etc. that are for private single users should not count toward usable open space unless the entire development is a single-family resident or duplex unit. Area underneath colonnades may count toward the usable space requirement.

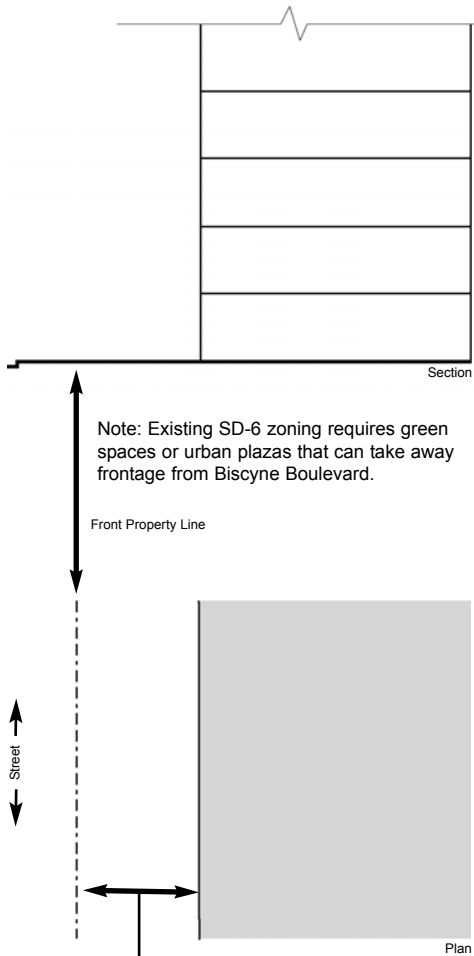
Given the urban nature of new development in Park West, it is assumed that most of the usable open space will be accommodated by providing a rooftop garden or deck. These areas should be accessible to the residents/occupants at a minimum of 10 hours per 24-hour day. Swimming pools count as open space. Any accessible open space above or below arcades and colonnades that is also above or within public rights-of-way may not count toward usable open space requirements. Usable open space may not be used for the storage of vehicles and boats, for driveways or accessways serving more than two vehicles, nor for mechanical equipment with footprints larger than 64 square feet.

This Special Area Plan recommends a minimum usable open space requirement of 15% of the net lot area to be consistent with neighboring zones. C-1 currently has a 10% gross area. SD-6 to the east of NE 2nd Avenue has a 15% net area requirement. SD-16 and R-4 zones to the west of NW 1st Avenue both have 15% gross area requirement.

Usable Open Space and Lot Coverage are regulated by each building. If a development has

more than one building, a building site or sites should be delineated on the site plan used solely to calculate area for open space and lot coverage. Only one building may be associated with a parking garage for calculation purposes on any one property. If more than one building is directly attached to the garage, the footprint of all buildings and the parking structure may be considered one building for calculation purposes. Covered or enclosed walkways or bridges from a parking structure to a building or between buildings shall not be considered as "directly attached."

**Current SD-6 Setbacks**  
(For comparison to new rules only)

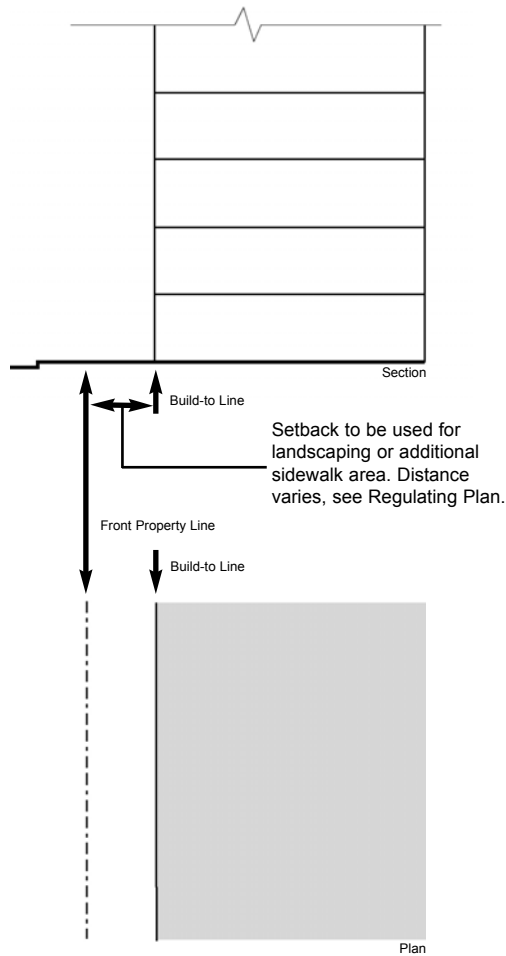


Note: Existing SD-6 zoning requires green spaces or urban plazas that can take away frontage from Biscayne Boulevard.

Front Property Line

Setback for Biscayne Boulevard is 15' minimum, 20' maximum except with optional colonnade that can be 28 feet back. For all other streets, 5' minimum.

**Street Frontages other than Biscayne Boulevard. There are two options: Without Colonnade** - or -



Build-to Line  
Setback to be used for landscaping or additional sidewalk area. Distance varies, see Regulating Plan.

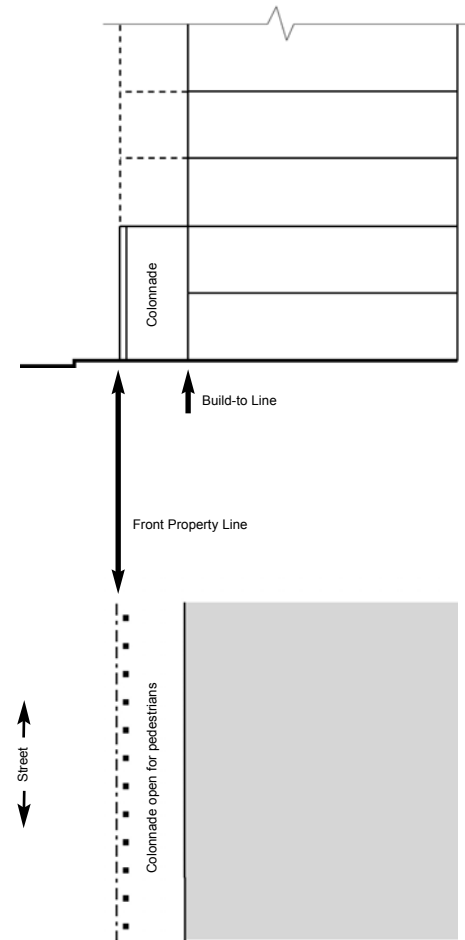
Front Property Line

Build-to Line

Plan

**With Colonnade**

Habitable space is allowed, but not required above colonnades and arcades, providing more leaseable space to the owner than implementing the option without the colonnades. Habitable space above the colonnade does not have to be as tall as the remainder of the building



Colonnade

Build-to Line

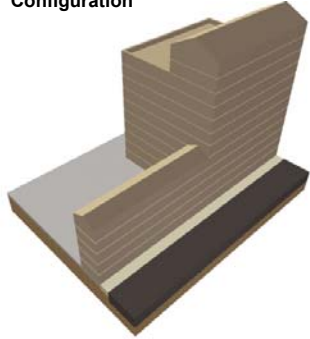
Front Property Line

Colonnade open for pedestrians

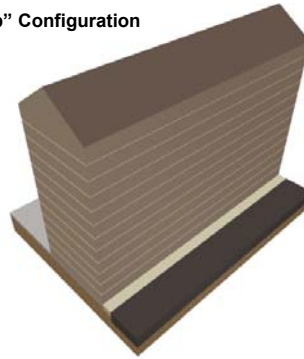
Street

Examples: (3-D)

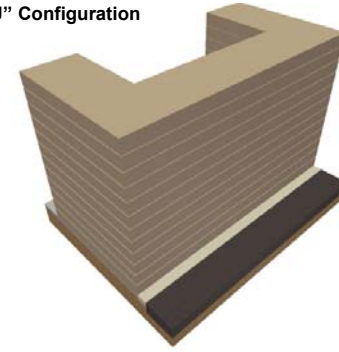
“L” Configuration



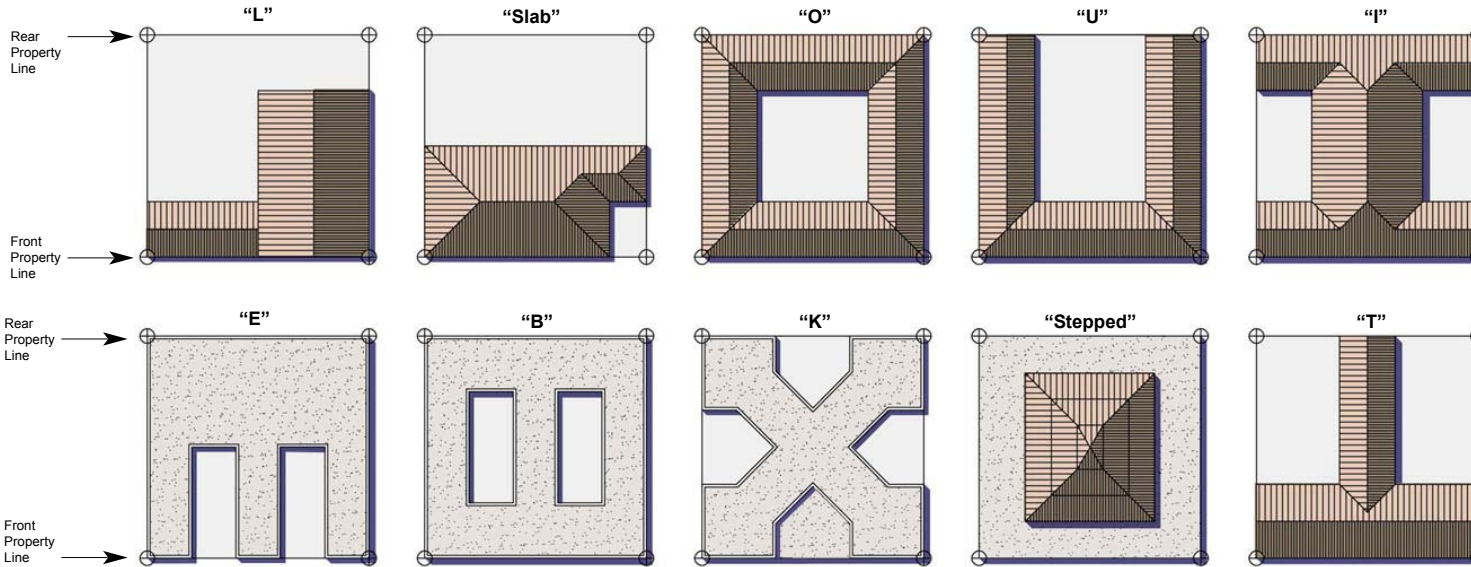
“Slab” Configuration



“U” Configuration



Acceptable building disposition: (Plan View)

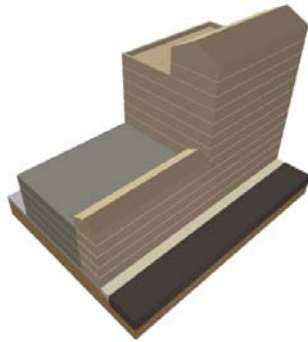


These building types are identified in the *Alphabetical City, Pamphlet Architecture #5* March 1980, by Steven Holl

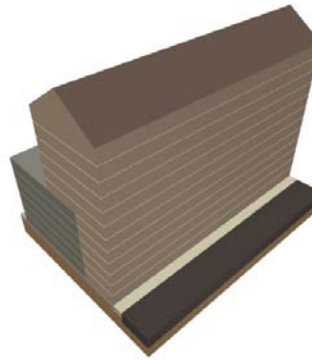


Examples with Parking Garages: (3-D)

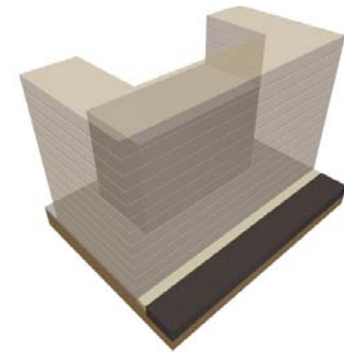
The objective of hiding parking garages in buildings can be accomplished with all of the permitted building dispositions.



The major portion of the building is a side-yard type. Parking deck is hidden from the street by a thin "liner" building attached to the garage.

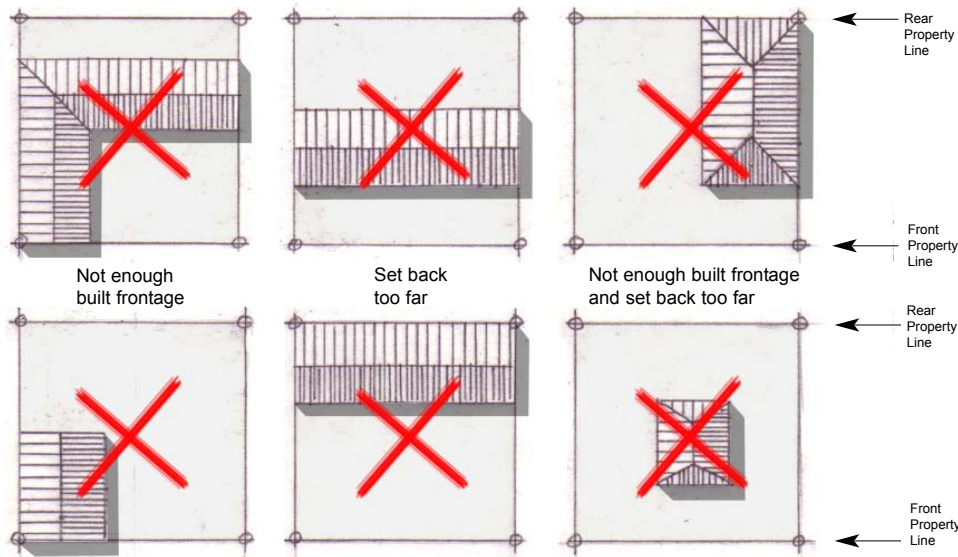


The building is oriented to the street. The parking garage is embedded into the building. A layer of habitable space separates the parking garage from the street.



The courtyard building "hugs" the parking garage. The roof of the garage becomes a natural location for usable open space.

Unacceptable building disposition: (Plan View)



## Building Configuration

### Building Floor Area Ratio

The floor area ratio (FAR) is a multiplier that dictates the amount of buildable area in a building. A developer multiplies the FAR number by the gross square footage of the property to get the maximum allowable square footage that can be built inside the building. See the “Glossary of Terms” for the definitions of gross area and net area.

These guidelines suggest changing the FAR limitations for property currently in C-1 zoning within Park West. The current FAR is 1.72. Instead of one fixed FAR, it should be varied, depending upon specific features or elements provided in the building's design. Below is a list of design features and their corresponding FAR bonus values. These amounts are accumulative.

- 1.72 Base FAR value, available to every property owner.
- 0.53 For building 100% for street frontage without the optional side setback for accessways.
- 0.50 For providing a colonnade that extends along the front of a building between 70 and 100% of the building's frontage (not the property's frontage)
- 0.50 For providing retail use on the ground floor for 100% of the building's frontage. This should require an executed pre-construction lease by a viable retail business or businesses for a two or more year period.
- 0.50 For providing operable doors every fifty (50) linear feet, on average, for the length of the ground floor frontage. Multiple doors side by side count as one. Measurements should be taken from vertical centerline of door(s) to centerline of door(s). An example tenant lease should be provided that includes a clause stating that these doors shall remain open, operable, and free of obstructions during normal business hours of operation.
- 0.50 For fifty (50) or more spaces beyond the minimal requirements provide these spaces will be accessible to the public either free of charge or for a fair fee.
- 0.25 For one-hundred (100) or more additional parking spaces beyond the minimum. This FAR bonus is in addition to the 0.5 for the first 50 spaces.
- 
- 5.00 TOTAL available FAR

### Building Height Standards

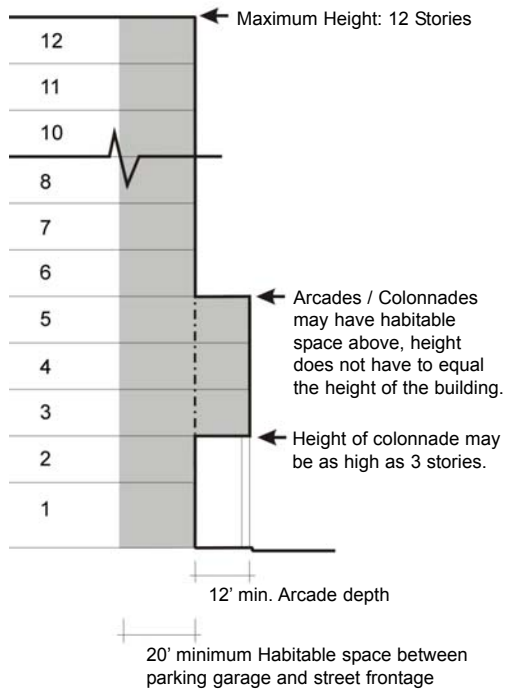
Building height is regulated by the number of stories, not a set height limit. A story shall be between eight (8) feet and fourteen (14) feet high, measured from floor to floor. Each building is allowed one story to exceed the fourteen (14) feet limit, but shall not be taller than thirty (30) feet. Mezzanines within the one higher floor (up to 30') shall not count as a floor provided the floor plate of the mezzanine does not exceed 25% of the floor plate below it. Floors exceeding the fourteen (14) foot height limit shall be counted as two or more stories, depending on how many increments of fourteen (14) feet they are spaced apart. The maximum building height for buildings along the Promenade shall be twelve (12) stories.

Parking structures have no height limit, except that they shall not be taller than the buildings hiding them from view. The parking structures levels need not align with the floor levels inside the building, however, accessibility requirements for the disabled still apply.

Arcades or colonnades are encouraged along the streets in Park West to collectively form a continuous shelter from the weather. As built, these may extend into the right-of-way

provided authorization from the entity that controls the right-of-way. The height of the arcades and colonnades shall be between one (1) and three (3) stories. Habitable space may be constructed above the arcades and colonnades. The number of stories of habitable space above the arcades and colonnades may vary. Any accessible open space above arcades and colonnades that is also above public rights-of-way may not count toward usable open space requirements.

First floor residential uses shall be raised a minimum of two (2) feet above the average sidewalk grade.

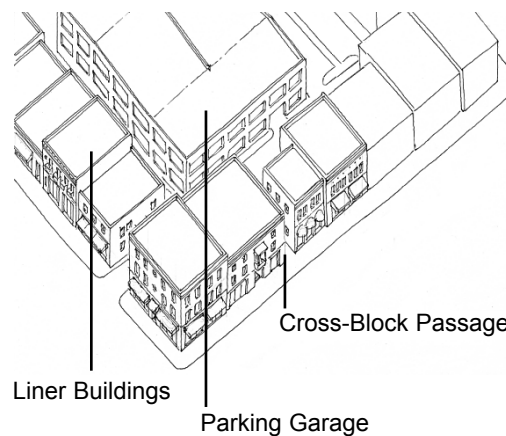


### Cross Block Passages / Liner Buildings

Cross block passages are encouraged to provide access to mid-block parking, and should occur on block faces a minimum of every 200 feet. Cross block passages should be at least (12) twelve feet wide, but should not be less than eight (8) feet wide, and are encouraged to line up to provide access all the way across the block, and to adjoining blocks.

Parking Structures must be lined by at least a twenty (20) foot deep layer of habitable space. Ground floors of garages may contain lobby entrances, entrances to parking, and should also contain retail space. Recommended liner building depths are thirty (30) to forty (40) feet for residential and office, fifty (50) to sixty (60) feet for retail.

A liner building is a narrow building which wraps around and is attached to a parking structure or a narrow, free-standing building at the edge of the property to hide the parking behind.



### Frontage Standards

All primary entrances to buildings shall occur facing the street. Buildings along the Promenade are required to have two entrances, one at the street and one that opens to the Promenade. Primary entrances should be enhanced architecturally from other entrances. Primary entrances to buildings shall not face parking lots, side courts, or occur inside parking garages.

Stoops, arcades, colonnades, awnings, marquees, balconies, and forecourts are all building elements typically used in the design of buildings. Not all of these elements will be utilized in the design of one building. However, if utilized on façades facing streets or facing the Promenade, the standards on the following two pages shall be used. The illustrations for each of the building elements are for illustrative purposes only and are not intended to dictate particular designs, materials or architectural styles.

Stoops, balconies, awnings, and bay windows may encroach into any setback. Awnings may encroach the public sidewalk to within eighteen (18) inches of the curbline.

Loading docks and service areas should be located only on rear alley or Promenade frontages.

a. Galleries, Colonnades / Arcades

*Depth* = 10 ft minimum from the build-to line to the inside column face.

*Height* = 10 ft minimum clear.

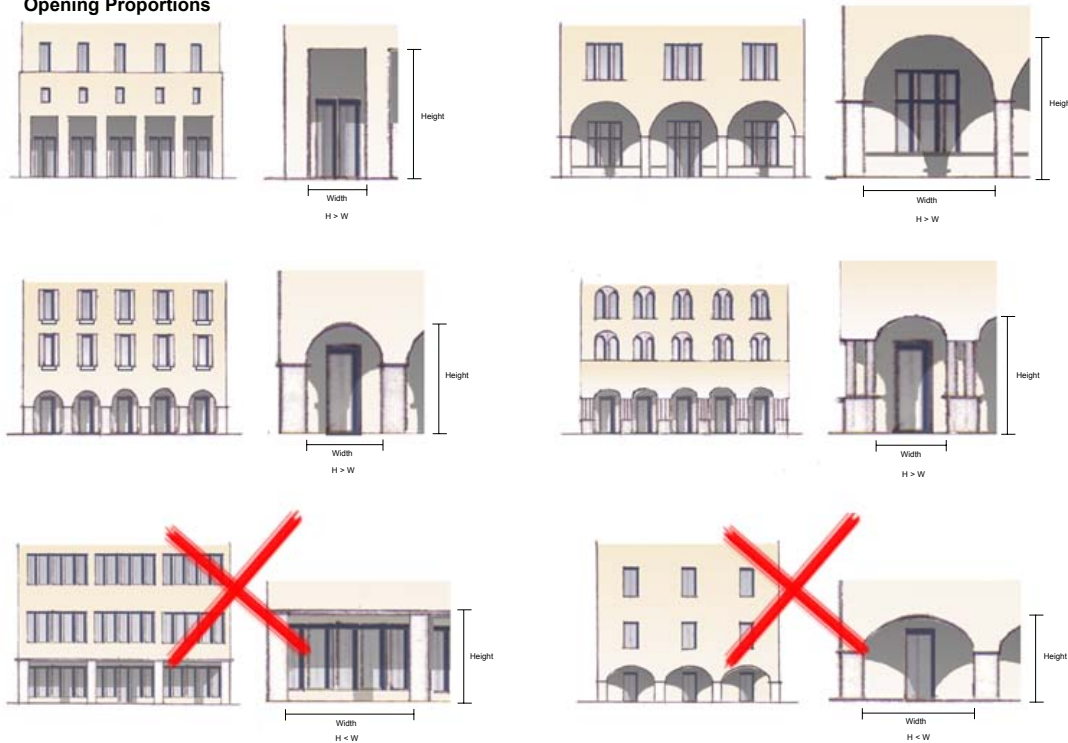
*Length* = 75-100% of Building Front.

Open multi-story verandas, awnings, balconies, and enclosed useable space shall be permitted above colonnades and arcades.

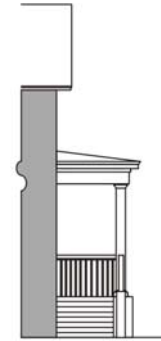
Colonnades and arcades shall only be constructed where the minimum depth can be obtained. Colonnades and arcades shall occur forward of the Build-to Line but shall not extend into a public right-of-way without permission of the controlling agency for that right-of-way.

On corners, colonnades and arcades may wrap around the side of the building facing the side street. Openings shall be of a vertical proportion where the height of the openings are greater than or equal to the width of the opening. The greater the difference the better the proportion will be.

**Opening Proportions**



b. Stoops:



*Depth* = 6 ft. minimum

*Length* = 5 ft. minimum

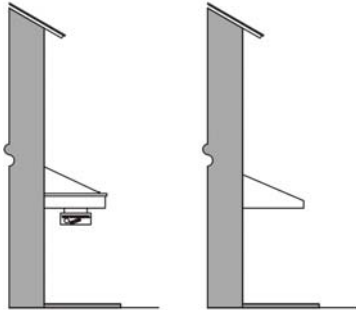
Stoops are permitted and may occur forward of the Build-to Line. Stoops may encroach within the right-of-way with approval.

Sidewalks shall have clear access for pedestrians. Stoops may be covered or uncovered.

Stoops shall not be used for primary entries unless ADA requirements are satisfied.



c. Awnings & Marquees



*Depth* = 5 ft minimum.  
*Height* = 10 ft minimum clear.  
*Length* = 25% to 100% of Building Front.

The above requirements apply to first-floor awnings. There are no minimum requirements for awnings above the first floor.

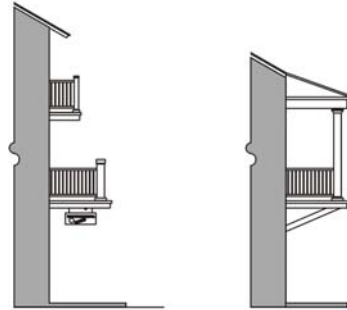
Marquees and Awnings shall occur forward of the Build-to Line and may encroach within the right-of-way, but shall not extend past the curb line.

Awnings shall be made of fabric. High-gloss or plasticized fabrics are prohibited.



Photograph by Bernard Consoboni

d. Balconies



*Depth* = 6 ft minimum for 2nd floor balconies.  
*Height* = 10 ft minimum clear.  
*Length* = 25% to 100% of Building Front.

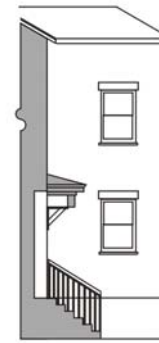
Balconies shall occur forward of the Build-to Line and may encroach within the right-of-way, but shall not extend past the curb line.

Balconies may have roofs, but are required to be open, un-airconditioned parts of the buildings.

On corners, balconies may wrap around the side of the building facing the side street.



e. Forecourt



*Depth* = 10 feet minimum to 30 feet maximum.  
*Height* = Determined by number of stories.  
*Length* = 5% to 33.33% of the length of the front building façade.

Forecourts are useable open spaces that recess behind the front plane of the primary façade of the building, typically for the entrance to the building, with walkways, terraces, and/or decorative landscaping. Forecourts shall not be used for the storage of vehicles, storage of any kind, nor for mechanical equipment such as ground-level electrical transformers, air conditioning compressors, pump houses, and cable and telephone cabinets.

Stoops, arcades, colonnades, balconies, and roof overhangs may occur inside forecourts, and need not meet the same requirements identified in this document, but only those frontage elements inside the forecourt. Primary entrances inside a forecourt should shield pedestrians from the weather.

## Building Type Standards / Land Uses

The goal for the new buildings on Biscayne Boulevard is to have a mix of uses either within the same building or in neighboring buildings. A list of standards and land uses for SD-6 can be found in Zoning Ordinance 11000, Section 604.4.

## Parking Standards

Current Parking and loading requirements for C-1 (the primary zoning category for the properties within the Promenade Special Area Plan) can be found in Section 401, Schedule of Zoning Districts, of the City of Miami Zoning Ordinance. These include:

For residential use, same as R-4:

1. One-family and two-family: Same as R-1  
Minimum two spaces per unit
2. Multifamily: Same as R-3:  
One (1) space for each efficiency or one-bedroom unit  
Two (2) spaces for each two-bedroom or three-bedroom unit  
Three (3) spaces for each 4 or more bedroom units  
Plus one additional parking space for every 10 units.
3. Child care: same as R-1  
One (1) space for the owner/operator and one (1) space for each employee. In addition to providing offstreet parking, such establishments shall provide safe and convenient facilities for loading and unloading clients including one (1) unloading space for every ten (10) clients cared for.
4. Community based residential facility: from Section 934.2.2.5:

One (1) space for each staff member and one (1) space for each four residents. Discounts are available, refer to Section 934.2.2.5 of the Zoning Ordinance.

Places of Worship: discounts are available, refer to Section 401.

For nonresidential use:

1. One (1) space per one hundred fifty (150) square feet of gross floor area for barber and beauty shops.
2. One (1) space per each five (5) fixed seats, and per each fifty (50) square feet of gross floor area for movable seats, for Bible study classes, meeting halls and places of general assembly, excluding classrooms and other areas not for general assembly.
3. One (1) space per every one hundred (100) square feet of gross floor area, for restaurants and banquet halls, bars, saloons, private clubs, and lodges.
4. One (1) space per every one-hundred (100) square feet of gross floor area, for game rooms, pool and billiard halls, and other similar establishments.
5. One (1) space per every two-hundred and fifty (250) square feet of gross floor area, for discount membership merchandisers.
6. Except as required above, one (1) space per three-hundred (300) square feet of gross floor area for other non-residential uses.

For lodgings: Two (2) parking spaces for every three (3) lodging units.

This Special Area Plan suggests that parking space dimensions, quantities required, and other aspects conform to the requirements specified in the City of Miami Zoning

Ordinance as shown above, as modified from time to time, but with the following additional requirements:

1. Buildings or developments with more than one use may utilize shared parking by providing a shared-parking analysis to be approved by the Director of the Planning and Zoning Department.
2. Parking structures shall meet building disposition and configuration requirements described elsewhere within these guidelines.
3. Parking lots shall be masked from view from streets and other public spaces by a liner building, street wall, and/or hedge. See “Additional Design Standards” for streetwall details.
4. Bicycle racks shall be provided, off street, to accommodate one bicycle for every ten (10) vehicular parking spaces.
5. Pedestrian entrances to all parking lots and parking structures shall be directly from a street or frontage line. Underground parking structures may be entered directly from principal buildings.
6. The vehicular entrance of a parking lot or garage shall be no wider than twenty-four (24) feet for vehicles plus an additional six (6) feet for pedestrian access, for a total of thirty (30) feet. If a wider pedestrian opening is desired, then that should occur in a separate entrance or accessway.

Parking garage signage and generic directional signs should conform to the City of

Miami’s signage ordinance.

Building designers should consider parking decks starting on the second or third floor to allow larger leaseable space on the ground floor for retailers.

One exception for the parking requirements is suggested for buildings existing at time of adoption of this Special Area Plan. Due to vacant occupancy of many of the buildings in Park West and the City's desire to see older buildings in the neighborhood revitalized, there should be no parking requirements for existing buildings. If existing buildings, with their current massing, are incorporated into a new development with add-ons or additional structures, the square footage of the existing building may omitted when counting toward parking requirements. This exception should not apply if additional habitable space is added directly above an existing building.

## Additional Design Standards

The purpose of the Additional Design Standards is to create an environment that is suitable to the desired character of the Promenade and downtown Miami; pedestrian-friendly and livable.

Exterior building walls shall be made of materials that are consistent with and celebrate local architecture as well as enhance the City image.

Exterior building wall materials: Heavier materials shall be below the lighter materials. For example, a stone or brick "base" to the building may have concrete block with stucco above it. Stone or brick above a stucco surface will look inappropriate.

A minimum of sixty-five percent (65%) of the building façade must be glazed on the ground floor for a commercial use. A minimum of thirty percent (30%) of the building façade must be glazed the second floor and every floor above. No glazing material may have a mirrored effect if it is placed on the Pedestal.

Streetwalls can be built along vacant properties at the sidewalk edge or to contain rear or side yards. Construction and finished materials should match the façade of the principle building, especially if they are attached in some manner to the principle building. Height: Ideally the height should be between three (3) feet and eight (8) feet above the side-

walk. The rule of thumb should be that the wall needs to be either low enough that someone cannot hide behind it for the purpose of surprising an innocent pedestrian or tall enough that someone cannot jump over it. Streetwalls should be constructed solid for the first three feet of height. Above three (3) feet, the streetwalls may be more transparent by using openings or adding metal pickets or wooden posts and balusters. The overall height should not exceed eight (8) feet or be lower than thirty (30) inches.

Outdoor storage and dumpsters shall be screened from view by a sight-obscuring wall made of material to match the façade of the principal building.

Doors and windows that operate as sliders are prohibited where visible from streets and other public spaces.

Windows shall use clear glass panes and shall be operable for at least the first three (3) floors. Glass should have a light tinting if any.

Operable doors are recommended at a maximum of fifty (50) feet spacing for commercial uses on any façades with frontage.

Curb Radii is to be the minimum necessary to comply with other lawful regulating agencies.

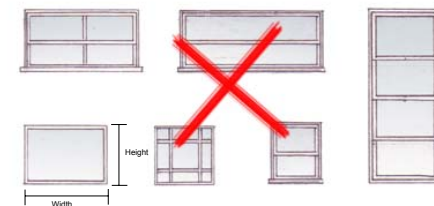
An expression line must be provided beneath 1st and 2nd floor, or between the 3rd and 4th floor when the Pedestal is more than three (3) stories in height.

### Window Proportions / Configurations

Vertical orientation preferred



Horizontal orientation to be avoided







### 7.3 Guidelines for Renovations

These guidelines are applicable for those who intend to renovate existing buildings in Park West. Renovations are defined as improvements to existing buildings that cost less than 50% of the building's value. Interior renovations shall conform to Miami's construction codes and are not addressed in these guidelines.

Typically, renovation projects include improvements to the exteriors of buildings. Any colonnades, arcades, balconies, awnings, roof overhangs, etc. that face streets shall meet the same requirements as specified in "Frontage Standards", in the Building Configuration subsection of Guidelines of Redevelopment, irrespective of where the existing building sits on its lot. Any proposed additions to existing buildings shall conform to the Redevelopment Guidelines.

#### Simple Improvements

When owners do not maintain their properties, they send negative signals to the community. The accumulation of litter and the deterioration of wall surfaces may discourage investment in the neighborhood, cause a perception that the neighborhood is unsafe, diminish pride that the residents may have for the neighborhood, and can cause stagnation or decline of property values. Conversely, as owners become diligent in the

maintenance of their properties, investors take notice, residents take pride in their properties and the public realm, and property values are more likely to increase. The following are measures that owners should take to improve their properties:

#### a. Clean up

Litter should be regularly removed from empty lots and from unbuilt portions of lots.

#### b. Repaint building walls

Because peeling paint and mildew stains create an appearance of neglect, walls should be painted or pressure cleaned regularly. Colors should be cheery, reflecting the flavors of our subtropical environment. Avoid painting large areas with dark colors. Dark paint colors may be used as accents.

#### c. Paint murals on blank walls

Where blank walls exist, murals could be painted. Murals should be thought of as a temporary treatment until either infill buildings hide the blank wall, or architectural treatments render the wall more articulate. When proposed, murals should comply with the City's Mural Ordinance.

#### d. Remove chainlink fences

Chainlink fences connote urban blight and trap litter along their base. Chainlink fences should be replaced with hedges, garden walls, fences, or pergolas.

e. Place dumpsters at the rear of the property  
Dumpsters and other private trash receptacles should be placed at the rear or side of the property. They should be screened by hedges, garden walls, and fences. If rear or side access is not available, dumpster may be in the front, but only if properly screened from view, preferably in an enclosed, air-conditioned room.

#### f. Maintain landscaping

Any grassy area should be cut at least once every two weeks. Grass should be edged along property lines. Owners should remove weeds.

#### g. Build walls and fences along property lines

Until buildings are constructed upon vacant lots, property boundaries should be defined by garden walls, fences, hedges, or pergolas. Such elements may be placed along property lines, at a height not to exceed ninety-six (96) inches (See "Additional Design Standards" for requirements for street walls). If used, pillars and posts shall average no more than ten (10) feet apart. Chainlink fences are not permitted along public property lines unless it is a temporary construction fence. They are permitted along property lines that divide two private properties, but only if they are not visible from the sidewalk at eye level. Only where necessary, walls, fences, hedges, and pergolas along street frontages shall angle

away from street intersections and driveways to avoid obstructing the sight visibility triangles or to provide the minimum eight (8) foot sidewalk width at the intersecting street.

#### h. Larger parapets

Where a building appears too short to provide a sense of spatial enclosure for the street, height should be added to parapets. As explained by the Site, Community, and Urban Planning section of Ramsey / Sleeper's *Architectural Graphic Standards*,

The height-to-width ratio of the space generates spatial enclosure, which is related to the physiology of the human eye. If the width of a public space is such that the cone of vision encompasses less street wall than sky opening, the degree of spatial enclosure is slight. The ratio of 1 increment of height to 6 of width is the absolute minimum, with 1 to 3 being an effective minimum if a sense of spatial enclosure is to result. As a general rule, the tighter the ratio, the stronger the sense of place, and often, the higher the real estate value. Spatial enclosure is particularly important for shopping streets that must compete with shopping malls, which provide very effective spatial definition. In the absence of spatial definition by façades, disciplined tree planting is an alternative. Trees aligned for spatial enclosure are necessary on thoroughfares that have substantial front yards.

#### i. Windows without security bars

Because they project the image of an unsafe neighborhood, security bars should be removed from windows and be considered for new installation. Instead of security bars, windows may have Lexan or other impact-

resistant glass. Roll-down shutters may not be used. Removal of bars should be required when owners propose changes to their storefronts or when other renovations are in excess of 50% of the value of the property.

## 7.4 Signage

General signage for buildings and businesses should conform to Article 10 of the City's Zoning Ordinance. Implementation of the Media Tower should comply with Article 10 unless a special signage ordinance is created for the district.

#### Special Signage Ordinance:

A special signage ordinance should be formulated and implemented that requires a higher level of animated and lighted signs, perhaps similar to the former SD-6 sign ordinance.

#### Directional Signs to Parking:

The City, with the collaboration of the Department of Off-street Parking (DOSPP), should formulate and implement a Parking Pathfinder signage system for the entire Park West, Overtown, and Omni areas.

## 5.0 Glossary of Terms

Definitions apply solely to these standards and supercede any definitions of similar terminology found in related materials.

**Accessway-** A drive or lane providing vehicular access from streets to parking or loading areas.

**Arcade-** A series of arches on the same plane, supported by columns, piers, or pilasters.

**Awning-** A protruding covering above a window on the outside to provide shade to the window and pedestrians.

**Base (of a Building)-** The bottom perimeter of a building that touches the ground.

**Block-** A combination of building lots, the perimeter of which abuts streets.

**Block face-** The right-of-way line or easement line which delineates a block edge.

**Build-to line-** A line established within a given lot indicating the margin along which the outer edge of a structure must be located.

**Build-to zone-** A zone established within a given lot indicating the area which the outer edge of a structure must be located.

**Building Frontage-** The vertical side of a

building which faces the primary space or any streets.

**Building Height-** A limit to the vertical extent of a building measured in stories above grade. Building height shall be measured in stories from the average elevation of the enfronting curb to the eave line.

**Buildings of Value-** A designation requiring that such buildings and structures may only be altered or demolished only in accordance with municipal preservation standards and procedures.

**Civic Use-** Premises used primarily for public education, cultural performances, gatherings and displays administered by non-profit cultural, educational, governmental, and religious organizations.

**Colonnade-** Series of columns in a straight line supporting an entablature, and typically roofed.

**Cornice-** Crowning, projecting, moulded horizontal top of a building or some part of a building or a wall.

**Expression Line-** A horizontal articulation detail usually between floor levels, such as a cornice, reveal, or a “jog” in material.

**Entablature-** The entire horizontal mass of material carried on columns and pilasters. Normally it consists of three main horizontal divisions, the architrave (essentially the lintel spanning between the columns), the frieze, and cornice.

**Floor Area Ratio (FAR)-** Specified in zoning that is multiplied by the gross land area to determine the maximum allowable floor area.

**Floorplate-** The total indoor and outdoor area of any given story of a building, measured to the exterior of the wall.

**Frontage-** The property line or lines of a lot which coincide with a right-of-way or other public open space line.

**Garden Wall-** A wall separating a courtyard garden from a public open space.

**Green Space-** An open space area not occupied by impervious surfaces and located at ground level.

**Habitable Space-** Building space whose use involves human presence with direct view of the enfronting streets or open space, excluding parking garages, self-service storage facilities, warehouses, and display windows separated from retail activity.

**Liner Building-** A building that is attached or detached, but directly in front of, a parking garage or large format commercial building. The purpose of a liner building is to add habitable spaces or residential and/or commercial uses with doors and windows facing streets.

**Open Space-** An outdoor, at grade space which is accessible to the public all or most of the time, including parks, plazas, squares, canal-walks, colonnades, courtyard gardens, paseos (when designed predominantly for pedestrians), and pedestrian paths and/or associated ornamental or shading landscaped areas.

**Parapet-** Low wall or barrier at the edge of a balcony, bridge, roof, terrace, or anywhere there is a drop that is a danger to persons falling.

**Paseo-** A cross-block, primarily pedestrian passage connecting one right-of-way or paseo

to another. Paseos shall be designed for pedestrian comfort, but may serve vehicles.

**Pedestal-** The bottom portion of a building which creates the street frontage.

**Plaza-** An open space with a majority of paved surface. Plazas are fronted with buildings that continue the adjacent street frontage requirements and uses.

**Retail Use-** Premises used for the exchange of services or goods.

**Setback-** The distance from base building line to the point where a building may be constructed.

**Square-** An open space surrounded by streets or other vehicular passages.

**Shared Parking-** Parking used by more than one (1) use or building.

**Story-** Volume between floors of a building. Entresols and mezzanines are intermediate floors between the main stories.

**Streetwall-** The vertical surface of a building or structure that faces public open space.

**Terminated Vista-** A designation requiring that a building be provided with architectural articulation of a type and character that responds to its axial location.

**Terrace-** Loggia or external usable space, e.g. roof garden.

**Tower-** The middle portion of a building above the pedestal

**Usable Open Space-** A landscaped or paved area or areas available to the general public or to the residents and occupants of a building.



Community Redevelopment Agency

49 NW 5 Street, Suite 100  
Miami, Florida 33128  
Telephone: 305.679.6800  
Fax: 305.698.6906  
[www.miami-cra.org](http://www.miami-cra.org)



444 SW 2nd Ave., 3rd Floor  
Miami, FL 33130  
Telephone: 305.416.1410  
Fax: 305.416.2156  
[www.ci.miami.fl.us](http://www.ci.miami.fl.us)

DOVER, KOHL & PARTNERS  
town planning

1571 Sunset Drive  
Coral Gables, FL 33143  
Telephone: 305.666.0446  
Fax: 305.666.0360  
[www.doverkohl.com](http://www.doverkohl.com)